

RESOLUTION NO. 056-07

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF TEMPLE TERRACE, FLORIDA, ACCEPTING THE RECOMMENDATION FROM THE TEMPLE TERRACE REDEVELOPMENT AGENCY; APPROVING THE UPDATE TO THE REDEVELOPMENT PLAN FOR THE TEMPLE TERRACE REDEVELOPMENT AGENCY AREA, WHICH UPDATE SUPERSEDES THE PREVIOUSLY ADOPTED REDEVELOPMENT PLAN.

WHEREAS, in accordance with the "Community Redevelopment Act of 1969", Section 163.330, et seq., Florida Statutes, and pursuant to the authority described in Resolution No. 116-99, adopted on September 23, 1999, the City Council authorized the preparation of a Finding of Necessity for Redevelopment, deemed the City's downtown business district as blighted, and approved the creation of the Temple Terrace Community Redevelopment Agency ("TTRA"); and,

WHEREAS, pursuant to the authority described in Resolution No. 56-01(m), adopted on May 15, 2001, the City Council approved the Temple Terrace Redevelopment Plan ("Redevelopment Plan") for an area comprising approximately 225.31 acres, generally located in the southwest area of the city limits, bounded roughly by the Hillsborough River to the south, 98th Avenue to the north, the City of Tampa to the west and Ridgedale Road to the east ("TTRA Area"); and,

WHEREAS, the Redevelopment Plan, in part, provides a strategy to eliminate those conditions of blight found to exist within the TTRA Area; and,

WHEREAS, pursuant to the authority described in Ordinance Nos. 1144 and 1145, adopted on May 17, 2005, the City Council created the Downtown Mixed-Use 25 ("DMU-25") future land use category, a new comprehensive land use plan category applicable to the TTRA Area allowing for a complimentary mix and range of land uses; and,

WHEREAS, the purpose of the DMU-25 future land use category is to establish, define, promote, and facilitate the redevelopment and enhancement of the City's primary and historical urban center, generally identified as its downtown central business district located near the Busch Boulevard/Bullard Parkway and North 56th Street intersection; and,

WHEREAS, the DMU-25 future land use category is intended to implement the City Council's vision for the downtown central business district, as expressed in the Redevelopment Plan, in order to ensure that all new development located within the DMU-25 designated property, and any substantial expansion of existing developments therein, create a compatible design of development within the TTRA Area; and,

WHEREAS, pursuant to the authority described in Ordinance No. 1146, adopted on May 17, 2005, the City Council created a Transportation Concurrency Exception Area ("TCEA"), as authorized by state statute, and related policies applicable to the TTRA Area, in order to encourage redevelopment and urban infill by reducing prior transportation concurrency restrictions through the use of a range of transportation options, including improved accessibility and mobility designs for new development; and,

WHEREAS, pursuant to the authority described in Ordinance No. 1173, adopted on May 16, 2006, the Temple Terrace Downtown Community Redevelopment Plan Overlay Zoning District ("Overlay District"), was amended to include design guidelines and standards to be applied during the review of all development within the TTRA Area; and,

WHEREAS, City Council further directed staff to update the Redevelopment Plan to reflect the ongoing comprehensive planning process undertaken within the last five (5) years as deemed appropriate by City Council, including additional flexibility for future development within the City; and,

WHEREAS, the City currently owns or controls certain real property within the TTRA Area, commonly referred to as "the southeast quadrant within the TTRA Area" and wishes to provide for desirable redevelopment therein, particularly in accordance with the provisions of DMU-25, TCEA and the Overlay District; and,

WHEREAS, the TTRA carefully reviewed City staff's recommended revisions to the Redevelopment Plan and thereafter approved a recommendation that City Council adopt the Update to the Redevelopment Plan ("Update"); and,

WHEREAS, the Hillsborough County City-County Planning Commission found the Update to be consistent with the City's Comprehensive Plan as required by law; and,

WHEREAS, in order to provide for the health, safety and welfare, and properly manage growth and redevelopment, adoption of the Update fulfills the foregoing goals and objectives of the City's Comprehensive Plan, including any particular portions thereof, as set forth in the Update.

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF TEMPLE TERRACE:

1. The above-described recommendations of the TTRA be and the same are hereby approved in all respects.
2. The Update, a copy of which is attached hereto as Exhibit "A" and incorporated herein by reference as if fully set forth herein, is hereby adopted in its entirety, and thereby supersedes any previously adopted redevelopment plan.
3. The legally authorized officers of the City of Temple Terrace are hereby directed to do all things necessary and proper to implement and make effective the provisions of this Resolution.
4. This Resolution shall take effect immediately upon its adoption.

PASSED, AND ADOPTED BY THE CITY COUNCIL OF THE CITY OF TEMPLE TERRACE, FLORIDA, this 19th day of June, 2007.

(Corporate Seal)

Attest:

Melissa E. Burns

Melissa E. Burns, MMC
City Clerk

Joseph A. Affronti, Sr.

Joseph A. Affronti, Sr., Mayor
Chairman of the City Council
CITY OF TEMPLE TERRACE, FLORIDA

CITY OF TEMPLE TERRACE

DOWNTOWN COMMUNITY REDEVELOPMENT AREA UPDATE



May 2007

U:/Community Dev/Common/CDD/
 Planning/Redevelopment/CRA Plan
 Update/WilsonMiller2006/
 City Council Meeting (May 15, 2007)



WilsonMiller

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- Ordinance No. 1145 changing the existing land use designation to the new “Downtown Mixed Use-25” (DMU-25) Future Land Use Category, adopted May 17, 2005.
- Ordinance No. 1146 creating a Transportation Concurrency Exception Area (TCEA) in the CRA, adopted May 17, 2005
- Chapter 29 Downtown Community Redevelopment Plan Overlay Zoning District, adopted May 16, 2005.

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1.0 Introduction

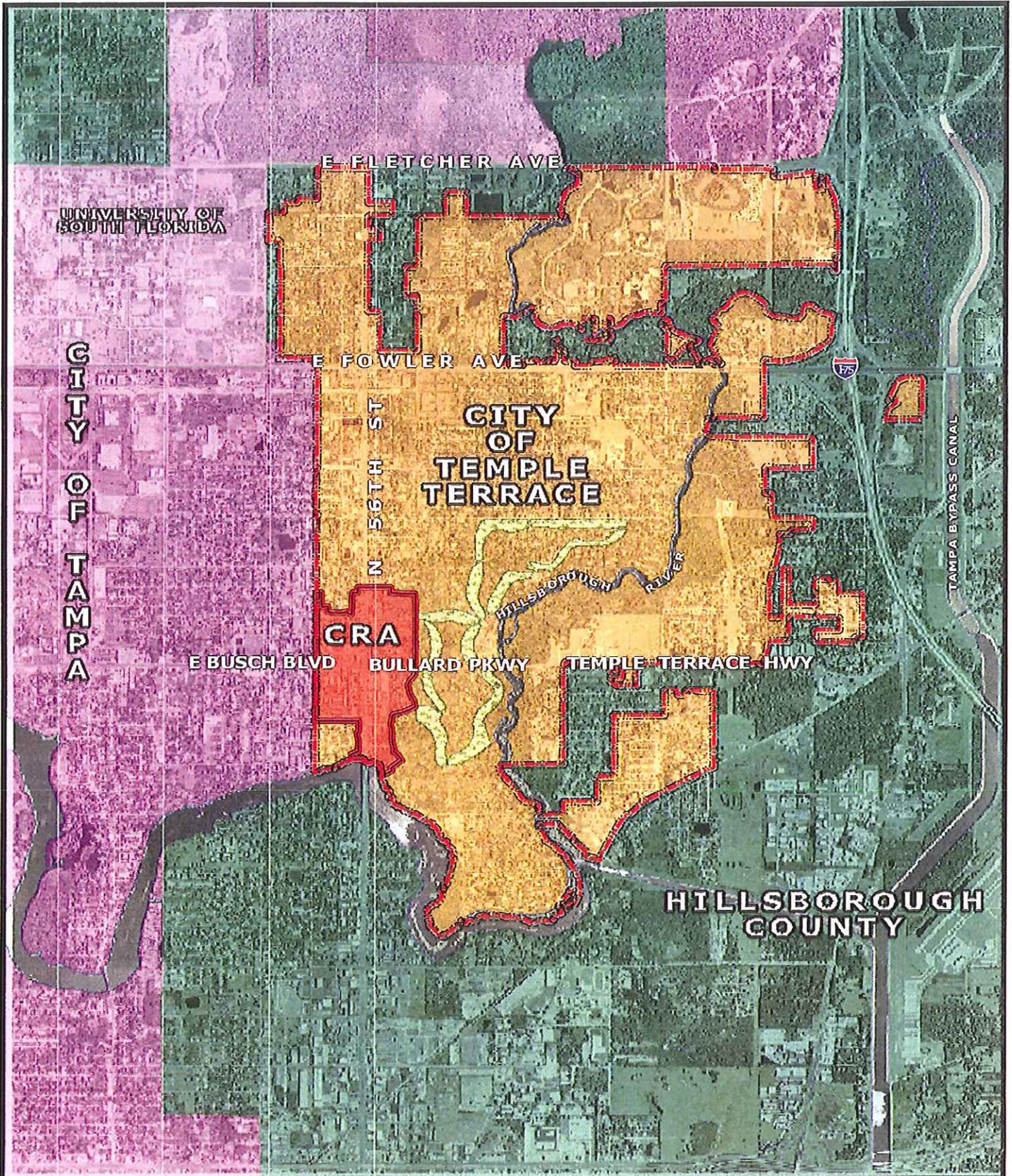
The City of Temple Terrace has recognized that significant areas of its downtown are in need of redevelopment. Community leaders have been working for many years to improve its business district, with E. Busch Boulevard and N. 56th Street at the core. In 1999, The City Council authorized the preparation of a Finding of Necessity for Redevelopment, deemed the downtown area as blighted in accordance with Chapter 163, Part III, Florida Statutes, and approved the creation of the Temple Terrace Redevelopment Agency (TTRA) Plan.

The purpose of the TTRA Plan is to provide a strategy to eliminate conditions of blight found to exist within the redevelopment area, as identified in the Finding of Necessity for Redevelopment. The Plan was prepared in compliance with Chapter 163, Part III Florida Statutes, including all applicable sections and supplements. It was approved and adopted on May 15, 2001. The City undertook this Community Redevelopment Plan Update to the Downtown TTRA Plan in late 2006 to reflect the planning progress that has been made in the last five years, and to permit additional flexibility for future redevelopment activities. This area of the City continues to face unique challenges and barriers to successful redevelopment.

The City created the Temple Terrace Redevelopment Agency (TTRA) to facilitate redevelopment, which allows for strategic investments to be made through the utilization of Tax Increment Financing (TIF). Since establishment of the TTRA, the City has amended its Comprehensive Plan and Zoning Code to establish, define, promote and facilitate the enhancement of the City's primary and historical urban center and its traditional downtown central business district. A master plan vision for downtown revitalization was completed. The City has been purchasing and consolidating parcels in the most blighted portion of the TTRA area, the area east of N. 56th Street and south of Bullard Parkway to the Hillsborough River, using funding mechanisms other than TIF.

The 2001 TTRA Plan contained design concepts including streetscape, landscape, signage, and architectural principles. These guidelines evolved to become detailed design and development standards of the Downtown Community Redevelopment Plan Overlay Zoning District, adopted as part of Chapter 29, Land Development Code, on May 16, 2006 (Ordinance 1173). As a result, those details were moved out of the 2001 Plan and into the Appendix in this TTRA Plan Update. Also in the Appendix is the Comprehensive Plan amendment, adopted on May 17, 2005 (Ordinance 1145). This amendment established goals, objectives, and policies to encourage greater density and intensity through mixed-use development, acquire vacant waterfront parcels for public purposes, protect existing neighborhoods, promote affordable housing opportunities, and implement master development plans.

The Downtown TTRA area comprises approximately 225.31 acres, and is generally located in the southwest corner of the City limits, surrounding the intersection of N. 56th Street and E. Busch Boulevard. It is bounded roughly by the Hillsborough River to the south, 98th Avenue to the north, the City of Tampa to the west, and Ridgedale Road to the east. The University of South Florida is located 2 miles to the northwest, and Busch Gardens Theme Park is 2 miles to the west. Temple Terrace is approximately 6 miles from Tampa's Central Business District. The limits of the Downtown TTRA area are shown on Maps 1.1 and 1.2.



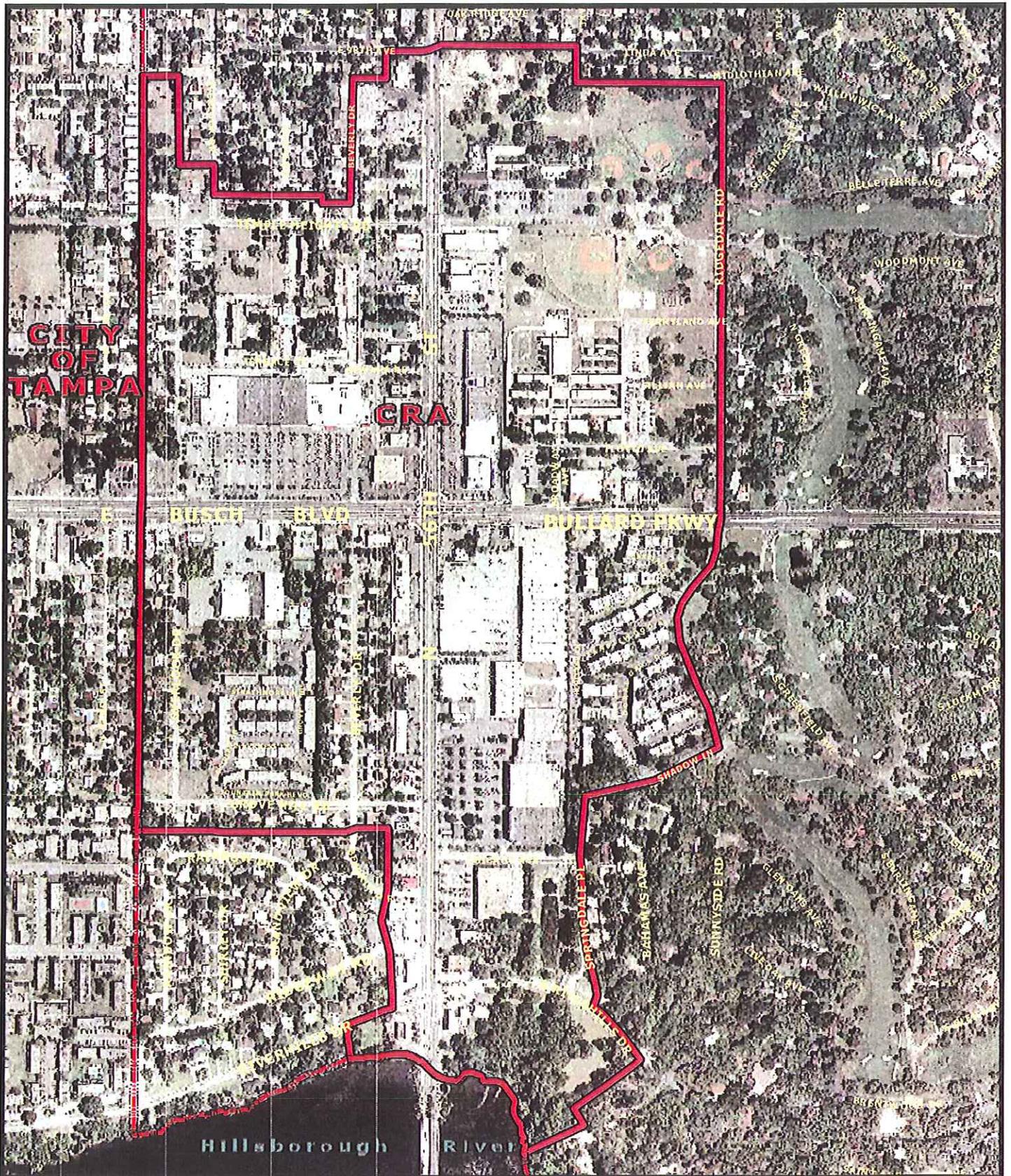
DOWNTOWN CRA PLAN UPDATE

MAP 1.1 LOCATION

1,000 0 1,000 2,000 Feet



GIS
Community Services
November 2006



DOWNTOWN CRA PLAN UPDATE

MAP 1.2 Aerial



GIS
Community Services
November 2006

2.0 Statutory Requirements

Any area proposed as a Community Redevelopment Area must meet the standards and requirements of the Community Redevelopment Act, Sub-sections 163.335 to 163.362, Florida Statutes. The process includes the preparation of a work program and schedule, a legal description of the area to be designated, a formal Finding of Necessity based upon survey and analysis, and the development of a Community Redevelopment Area (CRA) Plan.

The CRA Plan is based on the Finding of Necessity and proposes the methods by which those conditions will be alleviated. The CRA Plan must also be determined consistent with the City's adopted Comprehensive Plan by the Local Planning Agency. The Plan must also be either consistent with the existing zoning and land use intensities and densities in the area or recommend changes deemed appropriate to encourage the appropriate development. The Plan will propose a general work program and time frame within which public and private resources may be used to accomplish rehabilitation and redevelopment, and provide appropriate public resources. The Plan may also recommend land acquisition, disposal and redevelopment, if appropriate.

3.0 Regulatory Status

This section of the CRA Plan examines the regulatory environment that currently affects the land use patterns within the Downtown TTRA area. This includes the Future Land Use designations from the City of Temple Terrace Comprehensive Plan and current zoning districts that regulate the type, size, and placement of structures within the TTRA area. It is also important to examine recent planning efforts that have been undertaken within the redevelopment area. It is the goal of the TTRA to make the CRA Plan consistent with this Comprehensive Plan in order to advance the broader land use policies of the City. Recent planning efforts were also reviewed to ensure consistency with the Plan.

3.1 Community Redevelopment Agency

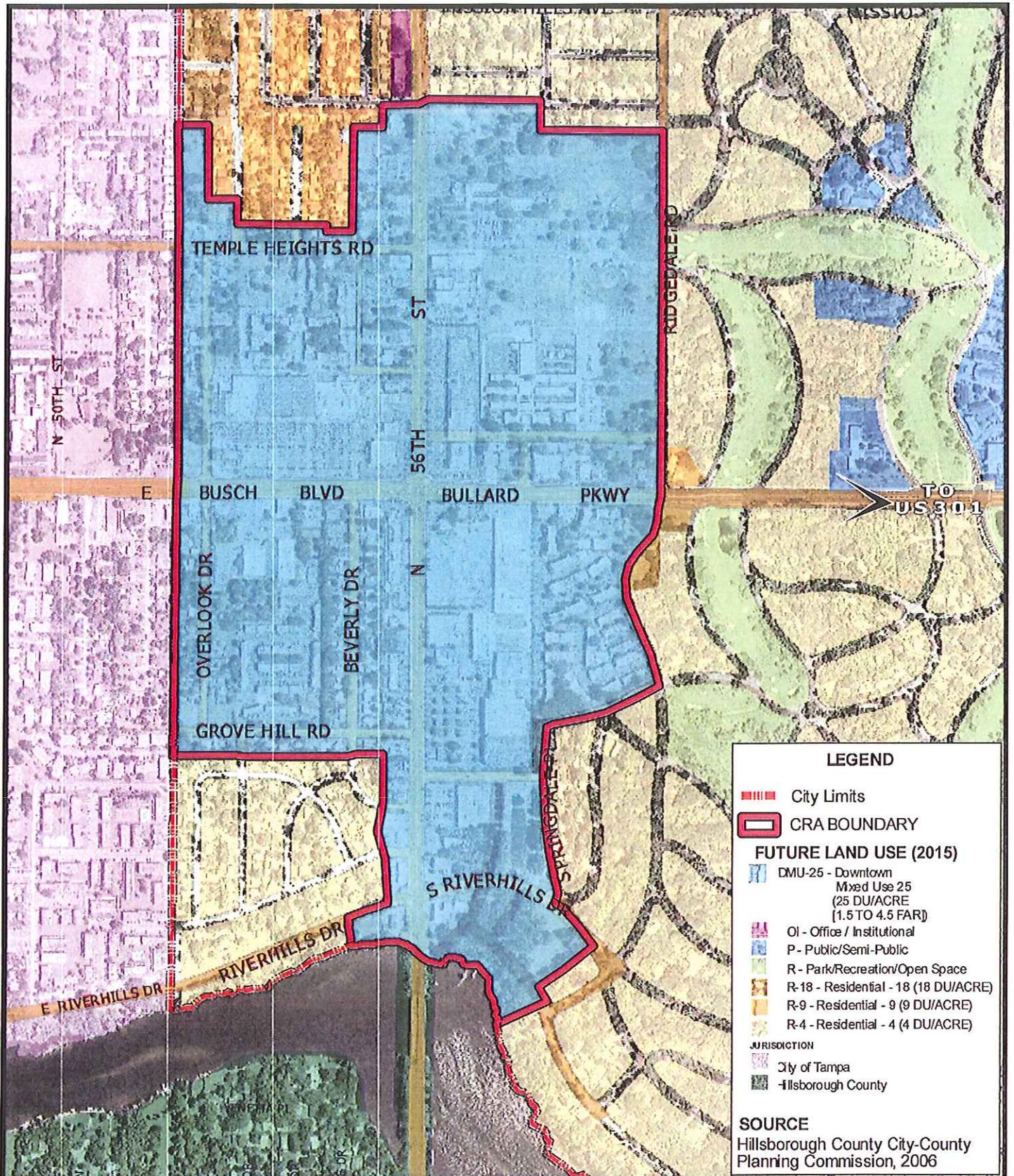
Per the Hillsborough County Board of County Commissioner's Resolution No. R00-067, approved on April 19, 2000, Hillsborough County delegated to the City of Temple Terrace the power to establish a community redevelopment authority pursuant to Part III, Chapter 163, Florida Statutes. Temple Terrace City Council by its own action also sits as the Temple Terrace Redevelopment Agency, and is responsible for administering programs and policies related to the City's Community Redevelopment Area.

The CRA Trust Fund was established on December 20, 2005, by Ordinance 1156. The Trust Fund over the last three years has generated \$379,369, which has not been spent to date. Past budgets were approved for the funds to be spent on relocation activities.

3.2 Comprehensive Plan

3.2.1 Future Land Use Element

The Future Land Use Element (FLUE) of the Temple Terrace Comprehensive Plan is designed to provide a framework for future growth by encouraging development/redevelopment of land within the City and an efficient use of existing infrastructure (See Map 3.1, Future Land Use).



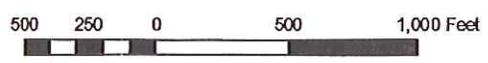
LEGEND

- City Limits
- CRA BOUNDARY
- FUTURE LAND USE (2015)**
- DMU-25 - Downtown Mixed Use 25 (25 DU/ACRE [1.5 TO 4.5 FAR])
- OI - Office / Institutional
- P - Public/Semi-Public
- R - Park/Recreation/Open Space
- R-18 - Residential - 18 (18 DU/ACRE)
- R-9 - Residential - 9 (9 DU/ACRE)
- R-4 - Residential - 4 (4 DU/ACRE)
- JURISDICTION**
- City of Tampa
- Hillsborough County

SOURCE
Hillsborough County City-County Planning Commission, 2006

DOWNTOWN CRA PLAN UPDATE

MAP 3.1 FUTURE LAND USE



GIS
Community Services
November 2006

The City's Comprehensive Plan was amended on May 17, 2005, to create a new Future Land Use category of "Downtown Mixed Use-25 (DMU-25)" and applied within the TTRA area. This new category allows consideration for new development of an urban character, and for a complementary mix and range of land uses. The purpose is to establish, define, promote and facilitate the redevelopment and enhancement of the City's primary and historical urban center and its traditional downtown central business district with special emphasis on uniformity of signage, site lighting standards and fixtures, façade treatment, color palette, architectural detail, building placement and orientation, landscaping, pedestrian amenities, and similar detailed design standards.

The DMU-25 category allows up to an average of 25 dwelling units per gross acre, with an absolute limit of 3,500 dwelling units within the entire TTRA area. Up to a maximum average of 1.5 floor area ratio (FAR) may be considered within the overall TTRA area, but a 3.5 FAR maximum average, and 4.5 FAR if structured parking is provided, may be permitted for individual projects. Allowable uses include: medium to high density multi-family residential, general commercial, office, public and semi-public, parks, recreation and entertainment venues, and institutional uses.

3.2.2 Transportation Concurrency Exception Area (TCEA)

An amendment to the Comprehensive Plan, approved May 17, 2005, allowed for creation of a Transportation Concurrency Exception Area (TCEA) as one way to encourage redevelopment activity in the TTRA area. In lieu of transportation concurrency restrictions, the TCEA is applicable to the entire redevelopment area and encompasses a range of transportation options for improved accessibility and mobility to facilitate urban infill and redevelopment. Specific strategies to encourage appropriate development patterns and reduce vehicular trips into the TTRA area are discussed in detail in Section 7, Proposed Plan.

3.2.3 Housing Element

The Housing Element of the Comprehensive Plan contains one goal, which is to provide a variety of adequate housing for all present and future residents of the City of Temple Terrace in order to serve the community's population in an orderly fashion. It is the City's objective to encourage redevelopment activities that do not displace the existing population, and when displacement occurs through public action, assure that reasonably located, standard housing is available at affordable costs.

The Downtown TTRA Plan will advance the goal and objectives of the Housing Element through the encouragement of mixed-use redevelopment that will provide rental and ownership opportunities for residents making a variety of incomes. The TTRA Plan envisions an urban neighborhood with a substantial increase in the number and type of residential units over what is currently located within the redevelopment area. The locations of existing low to moderate income residential complexes and their just market values are shown on Map 5.1.

3.3 Zoning

The City developed and adopted a new overlay district for the TTRA called the Temple Terrace Downtown Community Redevelopment Plan Overlay Zoning District. This is consistent with the Future Land Use category Downtown Mixed-Use 25 (DMU-25) described above. The current

zoning designations are for residential, office, commercial and public uses (See Map 3.2 Zoning). The existing zoning categories within the TTRA area include:

- R-10 (Single Family Residential)
- R-7.5 (Single Family Residential)
- R-MF (Multi-Family Residential)
- CO (Commercial Office)
- CG (Commercial General)
- PD (Planned Development)
- PDR (Planned Development Residential)

Regardless of these zoning categories, all rezoning petitions, new development and/or substantial expansion of existing development within any portion of the TTRA area must comply with the design guidelines and standards established by the Downtown Overlay District. The Overlay was adopted as an amendment to Chapter 29 of the Land Development Code as a set of detailed design and development standards to implement the goals of the DMU-25 Comprehensive Plan designation in keeping with the Downtown Temple Terrace Revitalization Master Plan and the vision for a recreated downtown, as described in Section 3.4 below.

The Overlay sets standards for maximum building heights, proportion of façade having continuous street frontage, and percentage of ground floor façade oriented to retail pedestrian traffic, all of which vary depending upon location of the project within a hierarchy of street types within the TTRA area. Also included in the Overlay are architectural design guidelines, required color palette, streetscape/landscape concepts, and standards for signage, site lighting, and accessory structures.

3.4 Revitalization Master Plan

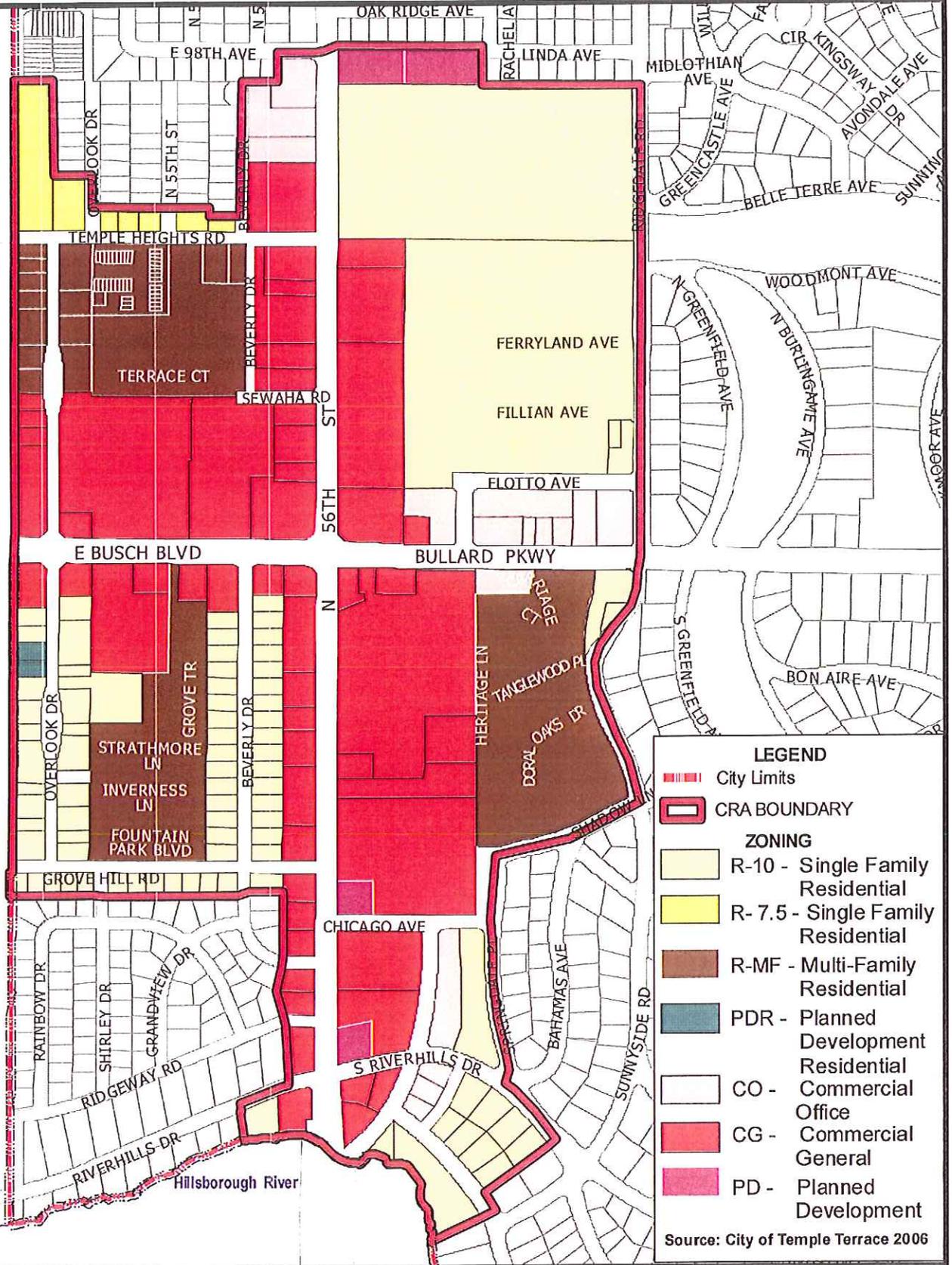
The Downtown Temple Terrace Revitalization Master Plan completed in January 2005 was the culmination of an extensive public participation process. During early 2004 a group of approximately 1,000 individuals, including City officials, civic leaders, residents and merchants, City staff, urban designers, landscape architects, traffic planners, civil engineers and economists, began to collect data and to work in a series of public workshops and planning charrettes, to create a vision document for the downtown. Their combined efforts became the Master Plan, which contained a challenging set of goals: creating a sense of place, a pedestrian-friendly environment, public squares and parks, a central open space, a “main street” as part of a new street grid, a unifying architectural identity, gateways, and a riverfront park. In addition, the plan encourages high-density mixed-use development, improving the character of N. 56th Street, concentrating civic uses, and linking to the Hillsborough River. A Design Code was also completed as a companion document to the Master Plan.

Development within the TTRA area will occur under the framework of the Updated Downtown Community Redevelopment Area plan, the existing Future Land Use designation, zoning districts, Downtown Overlay standards, and the vision documents for redevelopment.

CITY OF TEMPLE TERRACE

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- LEGEND**
- City Limits
 - CRA BOUNDARY
 - ZONING**
 - R-10 - Single Family Residential
 - R-7.5 - Single Family Residential
 - R-MF - Multi-Family Residential
 - PDR - Planned Development Residential
 - CO - Commercial Office
 - CG - Commercial General
 - PD - Planned Development

Source: City of Temple Terrace 2006

DOWNTOWN CRA PLAN UPDATE

MAP 3.2 ZONING



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4.0 Need for Redevelopment

The City of Temple Terrace stands at the threshold of a resurgence and is preparing to take steps to encourage investment and economic development that will improve the quality of life for its residents, both current and future. Redevelopment is necessary to create the downtown envisioned by the City's citizens, merchants, and visitors.

4.1 Neighborhood Character

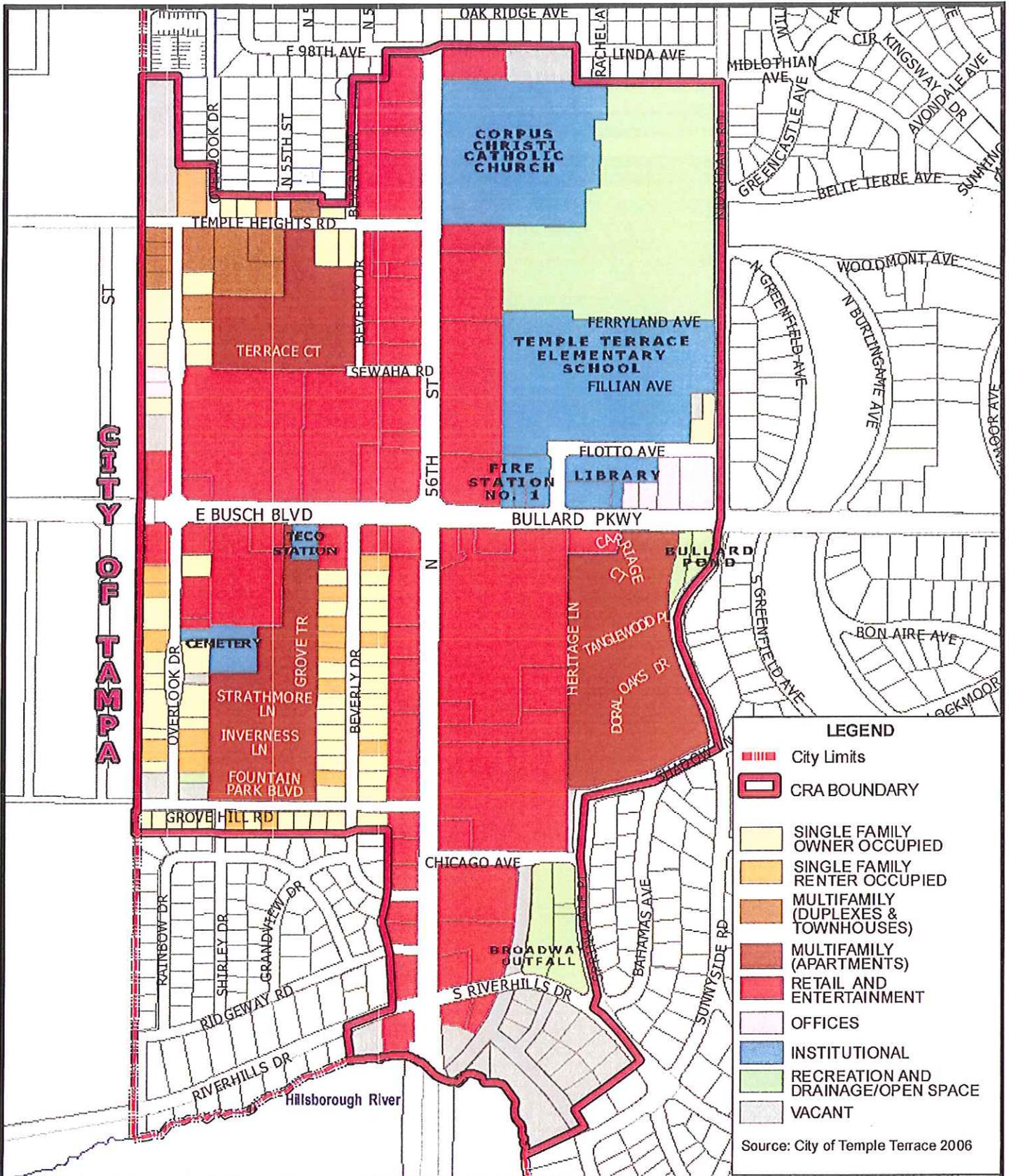
The City of Temple Terrace has initiated redevelopment planning for the commercial area surrounding the intersection of E. Busch Boulevard and N. 56th Street. Temple Terrace is a suburban scale community with a 2005 actual population of 23,879. A principal economic influence on Temple Terrace is the University of South Florida, located about 2 miles northwest of the redevelopment area. Major employers within the City are Verizon, Coca Cola, Florida College and offices located in Tampa Telecom Park. Busch Gardens is located on E. Busch Boulevard about 2 miles west of the redevelopment area, and Hidden River Corporate Park is off Fletcher Avenue north of the City boundary.

The TTRA area, shown on Maps 1.1 and 1.2, contains 225.31 acres including the City's southern and western gateways. It abuts the City of Tampa on the west and unincorporated Hillsborough County on the south. Development along its western boundary is consistent with the commercial uses in Tampa along E. Busch Boulevard. The southern boundary of the area is the Hillsborough River. Uses south of the river along the N. 56th Street corridor are predominantly mixed with multi-family residential, commercial and industrial uses. The development pattern outside the redevelopment area to the north is a mixture of low-density residential with some office uses. To the east of the TTRA area are single-family residential neighborhoods.

Connections to locations outside the TTRA area are provided by E. Busch Boulevard, which extends from western Hillsborough County. E. Busch Boulevard becomes Bullard Parkway within the redevelopment area, and changes function from an arterial street to a local collector. East of the Hillsborough River, Bullard Parkway becomes Temple Terrace Highway, which ends at Harney Road. N. 56th Street runs north from Dr. Martin Luther King, Jr. Boulevard south of the City to Fletcher Avenue, 2 miles north of the redevelopment area. N. 56th Street is the main north-south arterial within the City.

The N. 56th Street/E. Busch Boulevard/Bullard Parkway intersection is a historically important crossroads in the community. However, in recent years the transportation focus has shifted to Fowler Avenue, located one mile north of the TTRA area. With a direct connection to I-75, 3 miles east of N. 56th Street, Fowler serves as a principal east-west arterial.

The CRA is not a traditional downtown, but it constitutes the older commercial core of Temple Terrace that developed post-World War II to support the surrounding residential development. Land uses have changed little since that time and remain a mixture of retail, residential, office and institutional (See Map 4.1, Existing Land Use). A breakdown of existing land uses is included in Table 1.



DOWNTOWN CRA PLAN UPDATE

MAP 4.1 Existing Land Use



500 250 0 500 1,000 Feet



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**Table 1
Existing Land Uses**

LAND USE	ACRES	PERCENT
SINGLE FAMILY, OWNER OCCUPIED	13.57	6.02
SINGLE FAMILY, RENTER OCCUPIED	5.25	2.33
MULTI-FAMILY RESIDENTIAL	33.20	14.73
RETAIL AND ENTERTAINMENT	56.87	25.24
OFFICE	15.45	6.86
INSTITUTIONAL	39.81	17.67
RECREATION	8.45	3.75
VACANT LAND	13.42	5.96
STREETS	39.30	17.44
TOTAL	225.32	100.00

Source: Hillsborough County Property Appraiser, June 2006

4.1.1 Single Family Residential

Single family residential is not a predominant land use in the TTRA area, comprising only 18.82 acres or 8.35% of the total area. According to the County Property Appraiser data for January 2006, there are 83 single-family homes in the TTRA area mainly located on interior streets within the southwest and northwest quadrants, 56 (64%) of which are owner-occupied. The majority of the homes were built before 1960, and 62 (75%) of the homes are within the \$100,000 - \$200,000 range. Home sizes are approximately 1,200 square feet on lots of about 9,000 square feet, with overall density of approximately four units per acre.

4.1.2 Multi-family residential

Multi-family residential comprises 33.20 acres or 14.73% of the TTRA area. Map 4.1 shows the location of the multi-family apartment complexes. While there are a few duplex and small multi-family developments, by far the predominant multi-family development pattern within the redevelopment area is large-scale apartment complexes. The Brentwood Place Apartments, Doral Oaks Apartments, and The Gardens at Temple Terrace (formerly The Oaks Apartments) were built in the early 1970's. All three complexes are built at approximately 20 units per acre, a relatively high density for suburban communities. Together they comprise 560 units of housing, with individual units ranging in size from 850 to 1,350 square feet. Most units (90%) have two bedrooms. Two bedroom-two bath units make up 48% of the housing units in these three complexes.

The multi-family complexes are all two and three story "walk-ups" and have laundry and swimming pools on-site. Architectural styles are undistinguished, and none of the building exteriors, landscaping and outside amenities appear to have been significantly updated since construction.

While all three complexes have increased in taxable value since the inception of the CRA, County Property Appraiser's data for Doral Oaks and The Gardens at Temple Terrace indicate recent sales prices are falling far behind rising assessed values. The Gardens at Temple Terrace sold for \$2,446,600 in 1996, then sold for \$3,000,000 in 1998, but the last sale in 1998 was only 64% of its current taxable value of \$4,688,600.

Doral Oaks has nearly doubled in taxable value from \$5,745,283 in 2000 to its current \$10,289,800; however sales prices have fallen from a high of \$8,800,000 in 1984 to \$5,573,200 at its last sale in 2001, a price only slightly more than half (54%) of its current taxable value.

Brentwood Place is currently valued at \$6,757,300, a 240% increase over its value of approximately \$2,840,000 in 2000, and with its sale in 2005 for \$8,735,000 it is the only one of the three with a recent sale price in excess of its current valuation.

4.1.3 Retail

Retail land uses shape the character and activity of the TTRA area. There are two principal retail nodes: The Kmart and Publix shopping plaza in the northwest quadrant of the intersection of N. 56th Street and E. Busch Boulevard, and the Terrace Plaza and Sweetbay Supermarket shopping centers in the southeast quadrant. Existing and proposed land uses are discussed in detail in Section 7 of this Plan.

Retail and entertainment uses are the largest single category of land use in the TTRA area with 56.87 acres or 25.24% of the total land area. Categories of retail uses fall into the range of neighborhood and community commercial, convenience and household goods and services. Typical products offered for sale are auto parts, groceries, and general merchandise. City officials have noted a trend toward businesses which cater to families of moderate incomes. Businesses such as discount groceries, "dollar" discount stores and check cashing establishments tend to spring up in less stable socio-economic environments. There does not appear to be a large overall vacancy rate among the retail uses; however, there are several vacancies in the southeast quadrant.

There are a number of restaurants in the TTRA area, including several fast food and drive-through restaurants. Chains represented are Burger King, Checkers and several pizza chain restaurants. Vallarta's continues to cater to a lunch and dinner crowd. There are two restaurant/bars. One new restaurant, Jo-Jo's Grill and Seafood, has opened in the southeast quadrant. The bowling alley/roller hockey rink located in the southwest quadrant is the principal entertainment business in the TTRA area and offers an active recreation facility for youth and families, including a restaurant, bar, and video games.

4.1.4 Office

Office uses comprise 15.45 acres, or 6.86% and are located in all four quadrants of the TTRA area. The existing office square footage is not assembled into office parks or a coordinated office district. The three-story Bank of America is the tallest building in the area and includes the Chamber of Commerce office and other professional offices. Office space in the TTRA is oriented to small professional practices such as dental, insurance, attorneys, real estate brokers, and accountants. Terrace Piazza, an approximately 9,000 square foot office development is currently under construction at 9710-9730 N. 56th Street.

4.1.5 Institutional

Public facilities and institutional uses constitute 39.81 acres, or 17.67% of the TTRA area. These uses include a church, an elementary school, an active recreation park, fire station, library, electrical substation, and cemetery, most of which are concentrated in the northeast quadrant of the E. Busch Boulevard/N. 56th Street intersection. Other City-owned lands occupy most of the southeast quadrant, where additional institutional, cultural and recreational uses are planned for the south gateway area. Map 4.2 shows the locations of the public facilities.

4.1.6 Vacant Land

Vacant land comprises a small part of the TTRA area. The City purchased several vacant acres for expansion of cultural and recreational facilities, leaving only 13.42 acres of vacant land or 5.96% of total land area.

4.1.7 Streets

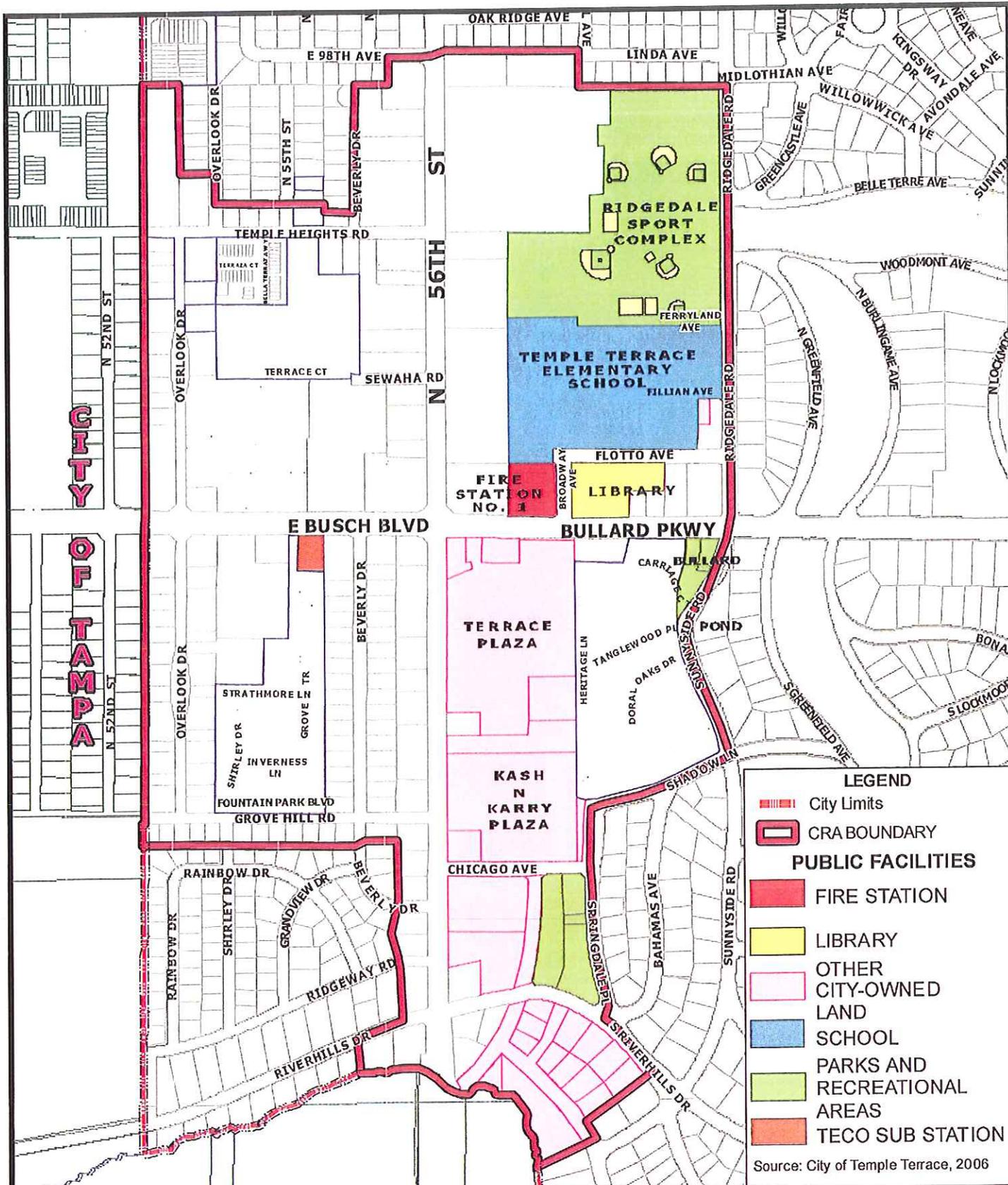
Streets and rights-of-way comprise 39.30 acres or 17.4% of total land area. The street system provides marginally adequate service to the existing land uses. A lack of connecting streets, high traffic volumes on arterials, and frequent curb cuts reduce the function of the street system and make pedestrian connections difficult. The redevelopment plan emphasizes the goals of re-establishing the grid street system and reducing curb cuts, improving visibility and safety of pedestrian crossings at N. 56th Street and Busch Boulevard, and development of streetscaping to provide tree canopy, street furnishings and pedestrian amenities. These goals are discussed in detail in Section 7.4.

4.1.8 Traffic and Transportation

The existing transportation system is depicted on Map 7.3. While an interior grid street pattern exists within the northwest and southwest quadrants of the TTRA area, only the southwest quadrant is effectively connected by public streets to the rest of the community. There is little street connection between the northwest quadrant and neighborhoods to the north. This lack of connectivity isolates intensive commercial uses such as the Kmart store, where exiting traffic uses Overlook Drive to access Temple Heights Road.

E. Busch Boulevard is a six-lane divided east-west roadway located west of N. 56th Street. It is a State maintained and owned facility, (designated SR 580) and has average annual daily trip volume of 40,500 vehicles and is operating at Level of Service (LOS) F between N. 52nd Street and N. 56th Street. East of the N. 56th Street intersection, the roadway is named Bullard Parkway/Temple Terrace Highway. It is a 4-lane divided, County-maintained facility, has an average annual daily trip volume of 29,100 vehicles and is currently operating at a LOS D.

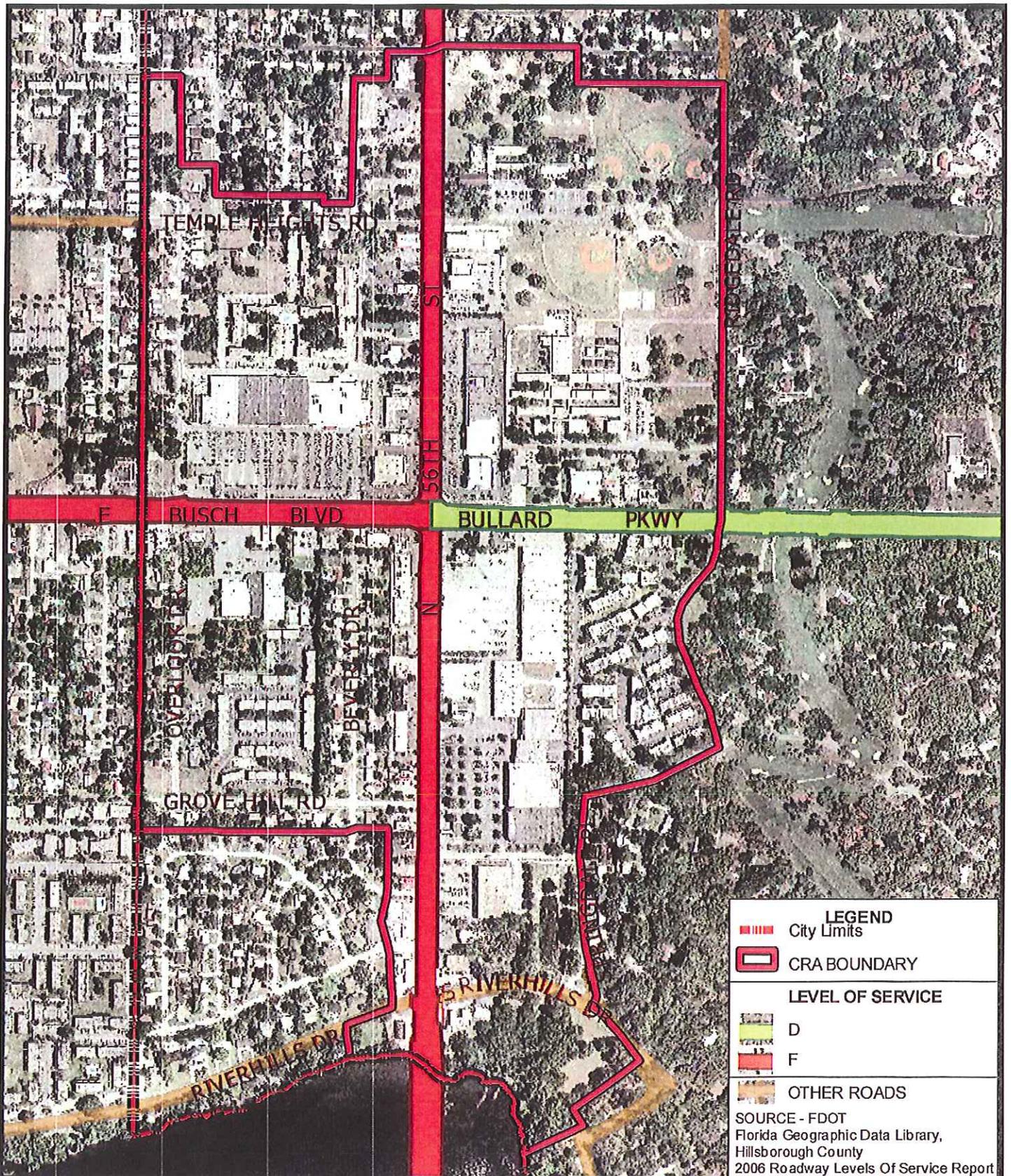
N. 56th Street (SR 583) is a four-lane divided north-south roadway that runs through Temple Terrace. It is a State maintained and owned facility, has average annual daily trip volume of 40,600 vehicles per day between Fowler Avenue and Hillsborough River. The roadway is currently operating at LOS F from the Hillsborough River to Fowler Avenue and from Fowler Avenue to Fletcher Avenue it has a LOS B. Heavy traffic volumes on E. Busch Boulevard and N. 56th Street are barriers to pedestrian and vehicular cross-movements. There are presently three signalized intersections (N. 56th Street/E. Busch Boulevard, N. 56th Street/Riverhills Drive, and N. 56th Street/Temple Heights Road) which support four-way traffic flow. Map 4.3 shows the roadway levels of service.



DOWNTOWN CRA PLAN UPDATE

MAP 4.2 Public Facilities





LEGEND

- City Limits
- CRA BOUNDARY

LEVEL OF SERVICE

- D
- F

OTHER ROADS

- OTHER ROADS

SOURCE - FDOT
 Florida Geographic Data Library,
 Hillsborough County
 2006 Roadway Levels Of Service Report

DOWNTOWN CRA PLAN UPDATE

MAP 4.3 ROADWAY LEVEL OF SERVICE



GIS
 Community Services
 November 2006

The northeast quadrant of the TTRA area lacks interior connector streets, a situation which burdens local residential streets with traffic. For example, Ridgedale Road is a residential collector which currently accommodates public use of the Ridgedale Sports Complex and the Temple Terrace Elementary School.

A curvilinear street pattern in the Riverhills subdivision in the southeast quadrant of the TTRA area provides four connections to the major road system at Ridgedale Avenue, Riverhills Drive, Chicago Avenue and Sunnyside Road. Proposed new streets, including a new Main Street in the downtown core as well as new streets serving the redevelopment area and connecting it to the adjacent neighborhoods are proposed. The street system approved for the TTRA area is discussed in detail in Section 7.4, Infrastructure Related Needs.

Hillsborough Area Regional Transit (HART) operates one local bus route # 6, and one express route # 23X along N. 56th Street in the TTRA area. The # 6 local bus runs from Downtown Tampa to the University Area Transit Center via N. 56th Street. Under an expanded schedule that began in November 2006, the #6 operates on weekdays every 30 minutes from approximately 4:30 a.m. to 9:00 p.m. and hourly from 9:00 p.m. to 1:00 a.m. Weekend operation is hourly from approximately 6:30 a.m. to 10 p.m. The Temple Terrace express bus Park and Ride lot is located at City Hall, and also stops at Terrace Plaza en route to Downtown Tampa. It leaves Temple Terrace at 6:35 a.m. and 7:05 a.m. and returns at 5:07 p.m. and 6:07 p.m.

4.1.9 Age of Structures and Structural Condition

The construction boom for property in the TTRA area occurred during the period 1960-1980. Table 2 displays the years that structures were built, based on Hillsborough County Property Appraiser's records.

**Table 2
Year Structures Built**

TIME PERIOD	SINGLE FAMILY RES	MULTI-FAMILY	INSTITUTION	RETAIL/ENTERTAIN	RECREATION	OFFICE	TOTAL	% OF TOTAL
BEFORE 1960	65	6	1	4	0	4	80	39.80
1960-1969	6	4	2	12	0	7	31	15.42
1970-1979	2	48	2	12	0	1	65	32.34
1980-1989	2		0	7	0	5	14	6.97
1990 TO PRESENT	2	0	1	6	0	2	11	5.47
TOTAL	77	58	6	41	0	19	201	100.00

Source: Hillsborough County Property Appraiser, December 2005.

Single-family residential structures generally appear to be concrete block and stucco construction. Multi-family residential buildings appear to be siding over frame. Based on a visual inspection, the condition of all structures appears to be sound.

4.1.10 Assessed values

Based on the 2006 tax roll, assessed values in the TTRA area are \$108,064,000. This is 5% of the total assessed value in the City (\$1,945,837,832 as of December 2006 according to County Property Appraiser records). It should be noted that assessed values by land use shown in Table 3 exclude undeveloped land, but the total City assessed value includes all land, both developed and undeveloped. Assessed values per land use are as follows:

**Table 3
Assessed Values by Land Use**

LAND USE	ASSESSED VALUE (000's)
SINGLE FAMILY RESIDENTIAL	\$9,380
MULTI-FAMILY RESIDENTIAL	\$22,776
RETAIL	\$48,341
OFFICE	\$10,899
INSTITUTIONAL	\$13,285
RECREATION	\$3,383
TOTAL	\$108,064

Source: Hillsborough County Property Appraiser, June 2006.

The highest assessed values for individual properties in the district are as follows:

**Table 4
Highest Assessed Values**

LAND USE	PROPERTY OWNER	ASSESSED VALUE
MULTI-FAMILY RESIDENTIAL	DORAL OAKS APARTMENTS 105 Sunnyside Road	\$10,289,800
MULTI-FAMILY RESIDENTIAL	BRENTWOOD PLACE TAMPA APARTMENTS 8741 Grove Terrace	\$6,757,300
RETAIL	CITY OF TEMPLE TERRACE 8859 N. 56 th St. (Terrace Plaza)	\$5,705,100
DISCOUNT DEPARTMENT STORE	K-MART 5400 E. Busch Blvd	\$4,487,180
RETAIL	CITY OF TEMPLE TERRACE 8837 N. 56 th St. (Sweet Bay Plaza)	\$4,869,900
SUPERMARKET	PUBLIX SUPERMARKET 5450 E. Busch Blvd.	\$4,814,600
MULTI-FAMILY RESIDENTIAL	GARDENS OF TEMPLE TERRACE APTS 5518 Terrace Court	\$4,688,600
SHOPPING CENTER (NE QUADRANT)	(Stores north of CVS Pharmacy) 9221 N. 56 th St.	\$4,049,600
POST OFFICE/RETAIL	CITY OF TEMPLE TERRACE 8633 N. 56 th St (USPS Annex, Dollar General, Just Brakes)	\$3,193,300
DRUGSTORE	WALGREENS 9202 N. 56 th St.	\$2,825,665

SOURCE - HILLSBOROUGH COUNTY PROPERTY APPRAISER – December 2006

An analysis of the assessed values in the district indicates that there is a strong concentration of values in highway frontage properties. With over 200 properties in the TTRA area, about 50% of the assessed value is concentrated in 10 properties. A strong economic stake is vested in a small number of owners. This concentration will enable the City to focus redevelopment planning on a relatively small number of owners and properties.

4.1.11 Infrastructure

Public water and sewer service are available to all parts of the TTRA area. According to the City staff, the systems are generally in good condition.

Most property in the TTRA area has adequate drainage from a standpoint of water quantity. The basin east of N. 56th Street has been significantly improved by the construction of the Broadway outfall project which included the installation of a continuous deflective separation (CDS) unit and detention pond. Drainage west of N. 56th Street is provided by a FDOT collection system with pipes that increase in diameter to a maximum of 60" at the outfall to the Hillsborough River. The FDOT also has installed a CDS unit at the River which provides some water quality improvements for the basin west of N. 56th Street. At the present time, there is sufficient capacity to drain the property along N. 56th Street, most of which has a high percentage of impervious surface coverage (see Section 7.4.1, Storm Drainage).

East of N. 56th Street, properties drain to the rear of the Terrace Plaza shopping center and then to an open drainage area west of Springdale Place. A large stormwater pond, located west of Springdale Place and south of Chicago Avenue, consumes 3.3 acres within the TTRA area.

The TECO electrical substation at 5301 E. Busch Boulevard was built in 1982 and occupies approximately one-half acre in the southwest quadrant.

4.1.12 Appearance and Design

The principal impression conveyed by the TTRA area is that of a group of 1960's era, unrelated commercial buildings with no dominant design theme. Because the area developed around the intersection of two major arterial roads, it does not have any of the visual hallmarks of a traditional downtown. The most prominent visual features are the roadway and adjacent parking lots. The "modern" architectural style of the mid-20th century is expressed by flat roofs, horizontal building massing, recessed doorways and shop windows, and limited building ornamentation. When decorative features were applied, they were likely to be in a large scale, such as the arches along the lower facade of the Bank of America building.

The appearance of most of the buildings varies, with some in above-average condition. There are several examples of pleasing architecture and good property maintenance, notably the Library building and the office building at the northeast corner of Ridgedale Road and Bullard Parkway.

With the expansion of the Kmart and the construction of a new Publix supermarket, and construction of a Walgreens and Varsity Cleaners, the northwest quadrant of the E. Busch Boulevard/N. 56th Street intersection had a much-needed face-lift. The remaining commercial properties need modernization and upgrading of parking areas, landscaping, facades and signs. Additional improvements that are needed are placement of new buildings close to the street edge, integration of landscaping with building design, and removal of visual clutter such as overhead power lines, numerous highway directional signs, and large business signs.

4.2 Factors Determining Blight

The Finding of Necessity report (September 10, 1999) concluded that there were sufficient blighted conditions present in the TTRA area to justify the designation. The Community Redevelopment Area Plan and the creation of a Tax Increment Financing District followed appropriate local administrative action. Many of the conditions of a "Blighted Area" according to Chapter 163, Part III, Florida Statutes, are applicable to the Temple Terrace Redevelopment Agency.

The following conditions of blight were found within the TTRA area:

- Predominance of defective or inadequate street layout
- Faulty lot layout in relation to size, adequacy and accessibility
- Unsafe and unsanitary conditions
- Deterioration of site and other improvements
- Inadequate and outdated density patterns
- Diversity of ownership

These conditions prevent or substantially impair the orderly growth and development or redevelopment by any entity.

4.3 Development Environment

In the TTRA area, many of the stores in the retail mix serve the needs of growing families -- businesses like Kmart, Walgreens, Hollywood Video and services such as the public library. Instead of boutiques or other specialty retail, retail uses serve needs at the lower end of the market, such as furniture rental, dollar stores, and check cashing stores. The growth of this retail sector generally indicates a demographic shift to more transient and less financially secure populations, although there remain general business and retail services that meet the needs of the more affluent members of the community. Retail vacancies occur mainly in the southeast quadrant. The additional movement of Publix to the northwest quadrant and CVS Drugstore to the northeast quadrant has increased vacancies in the southeast quadrant.

Property maintenance standards are not uniformly high throughout the TTRA area. Signs of decline such as cracked and uneven parking areas, and neglected or overgrown landscape areas and sidewalks have created a substandard appearance in some parts of the TTRA area.

The TTRA area is far from meeting its development potential envisioned in local planning documents. With the encouragement of the City, the development environment in the area is promising. Continued market demand for a more diverse urban environment for residents and businesses is anticipated, and the City is actively planning to meet that demand. A significant national trend in recent years has been for people to move back into central city areas, seeking urban, walkable neighborhoods with dependable and efficient transit services, near cultural facilities, parks, restaurants and shopping. Temple Terrace is committed to welcoming that demand to create an attractive, mixed-use, pedestrian-friendly downtown.

5.0 Neighborhood Impact

5.1 Land and Building Acquisition

The City already owns several key properties in the TTRA area, and a former gas station on the N. 56th Street frontage has been demolished. Implementation of this CRA Plan may necessitate additional land acquisition and building demolition. It is intended that private market land and building transactions be used to the maximum extent possible in the implementation of this Plan. Acquisition of real property through condemnation, which has been severely limited by recent State legislation, or other lawful means, to assist or facilitate the redevelopment process will only be utilized when all concerted efforts to acquire key properties and/or buildings through private transactions have failed.

5.2 Affordable and Replacement Housing

State statutes require that the City address affordable housing in the TTRA area. There are more than 60 single-family homes appraised for less than \$150,000 currently located within the TTRA area. In addition there are approximately 500 subsidized rental units within one mile of the TTRA area. Map 5.1 shows the locations of these single-family homes, and Map 5.2, the locations of Subsidized Residential complexes.

Implementation of the Plan is expected to create a range of housing and job opportunities for current residents and people relocating to the TTRA area. During necessary redevelopment activities initiated by the City, property owners will be offered fair market value for their homes. In addition, the Hillsborough County Community Development Department offers programs for rental assistance and first-time homebuyers. One avenue to assist displaced residents will be the opportunity for joint development of new housing units. Through State and/or Federal programs, it may be possible to build new units in the redevelopment area.

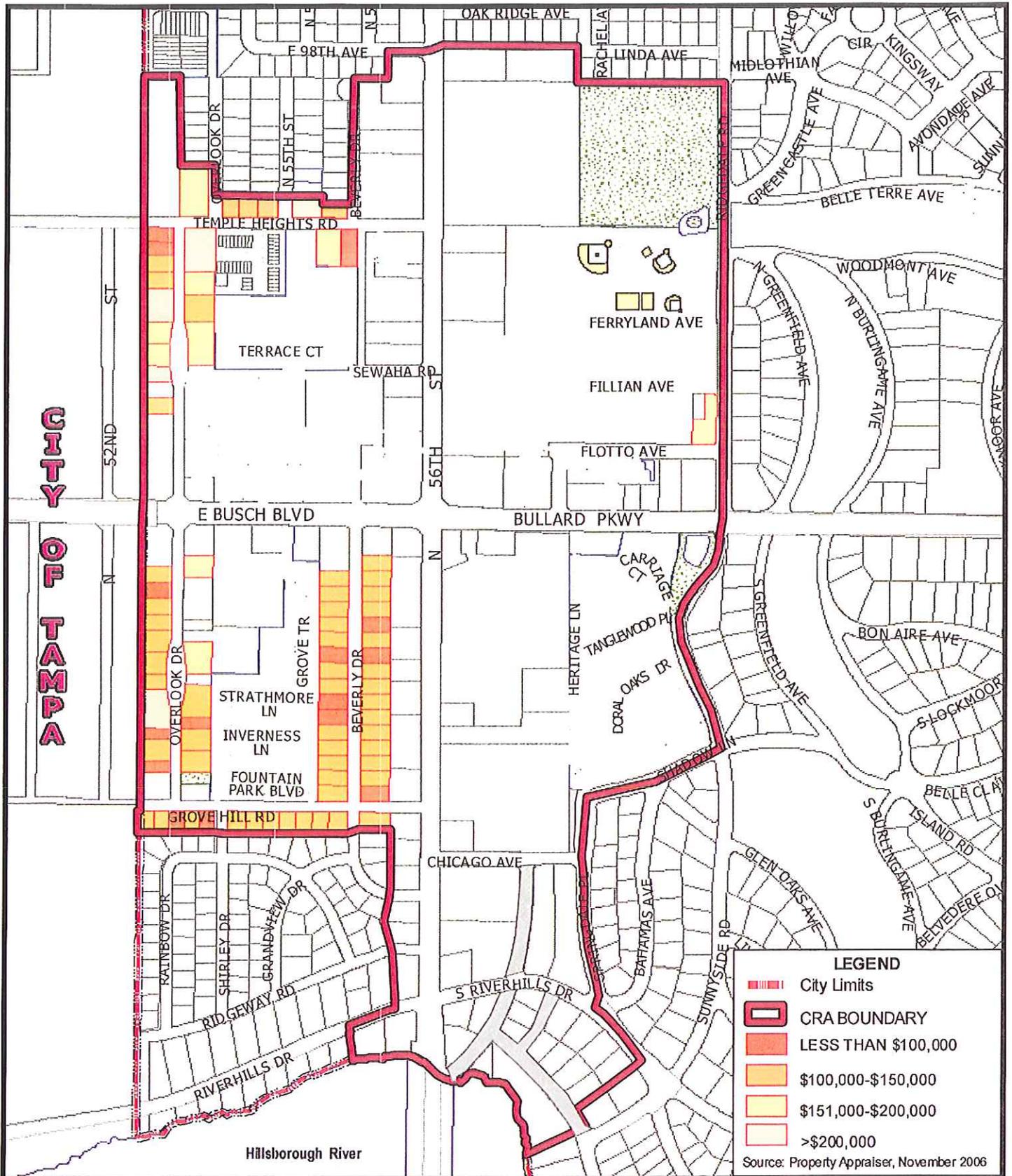
The redevelopment of the City owned land south of Bullard Parkway and east of 56th Street will include a mix of housing opportunities, including townhomes and live/work units. This residential product will be varied in size and configuration, and will offer an opportunity for affordable/workforce housing.

Furthermore, as is discussed in Section 7.3.2, the City owned property in the northeast quadrant of the TTRA offers a potential area for the possible location of affordable residential units. One potential scenario could include a real estate transaction wherein the City agrees to sell the property in conjunction with the execution of a Developers Agreement, which would require the provision of a certain percentage of affordable units.

Ultimately, from the data shown in Map 5.2, the City identifies that the substantial number of subsidized residential units available within one mile of TTRA area boundaries is sufficient to meet market demand, and, therefore no subsidized housing is planned within the TTRA area.

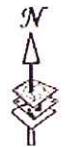
5.3 Transportation

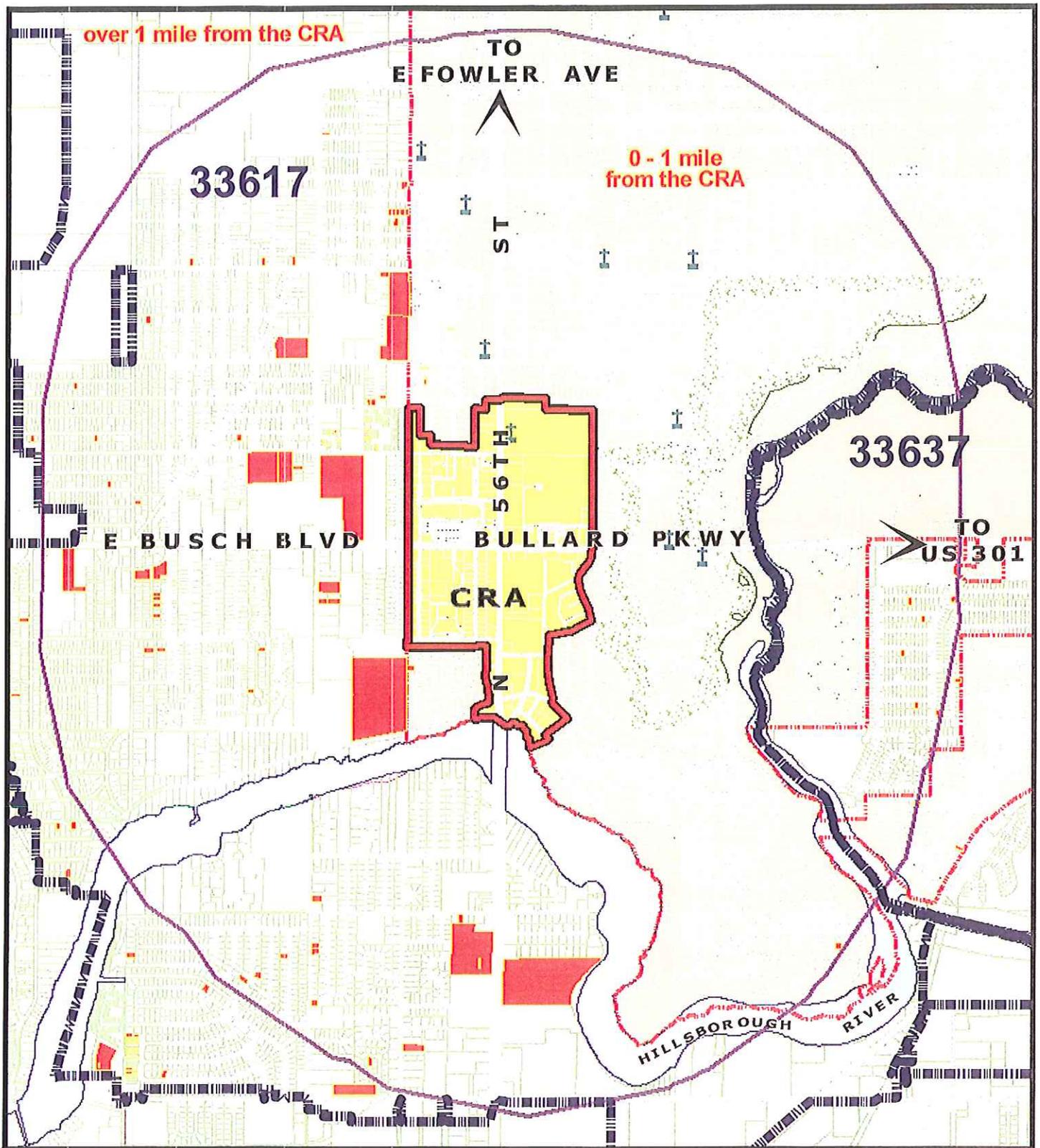
The Finding of Necessity report documented a significant need for improved transportation facilities within the TTRA area boundaries. Improvements to the transportation system will be undertaken both by the private sector and the TTRA through the use of gas tax revenue, TIF, and other sources of funding from local, State and Federal agencies.



DOWNTOWN CRA PLAN UPDATE

MAP 5.1 Low To Moderate Priced Single Family Residential Units





DOWNTOWN CRA PLAN UPDATE

MAP 5.2 Subsidized Residential Units



-  CRA BOUNDARY
-  SUBSIDIZED HOUSING
-  1 MILE BUFFER

- 0 1000 2000 Feet
-  City Limits
 -  Zip Code

Jurisdiction

-  Hillsborough County
-  City of Tampa
-  City of Temple Terrace

Data Sources

1. HUD - Tampa Field Office
2. Tampa Housing Authority
3. Hillsborough County Section 8 Department
4. Florida Housing Finance Corporation (Tallahassee)



GIS
Community Services
November 2006

As the Downtown TTRA area transforms into a denser, urban neighborhood, mass transit will become an integral part of the area's success. Mass transit will be required to support the higher densities and intensities called for in the Comprehensive Plan. Other transportation improvements in the area may include the provision of sidewalks, bike lanes, and increased road linkages and pedestrian connections to adjacent areas. These changes will create an improved transportation network, which will allow for easy movement for residents and visitors, and improved access for future businesses.

5.4 Community Facilities

The TTRA area's central location, together with infrastructure improvements that are scheduled as part of the redevelopment effort, insure provision of access to public water and sewer and the same high level of fire and emergency services and police protection as is available to all citizens of Temple Terrace. Access to public facilities, parks and open space is an integral part of the overall redevelopment effort in creating a pedestrian-friendly downtown.

New commercial and residential construction and enhancement of the drainage facilities at the south end of the TTRA area adjacent to the Hillsborough River will receive special attention to provide appropriate mitigation of any adverse effects upon a natural resource of great importance to the entire region. Specific approaches to preservation of natural resources to be employed during site plan development for new projects are discussed in Section 7, Proposed Plan.

The TTRA area includes the approximately 10-acre Ridgedale Sports Complex, located along the northeastern corner of the TTRA area. Currently, this park includes five ball fields and a playground. Throughout the life of the CRA Plan, TIF and other readily available funds may be utilized to improve the sports complex, and is expected to fund additional park and public open space facilities, creating attractive public assets that will be appreciated and utilized by residents from the TTRA area and throughout the City of Temple Terrace.

5.5 School Population

The TTRA area is currently home to Temple Terrace Elementary School, and is owned, operated and maintained by the School District of Hillsborough County. The school was built in 1955, and sits on a 20-acre property. Attending students are 47% African American, 22% Hispanic, 21% White, 2% Asian, and the remaining are of other ethnicity. The school is currently at 80% capacity, with a State grade of "C". The anticipated high-quality residential development in the Area is expected to have a positive impact on the school. Any residential development that occurs within the TTRA area will be subject to school concurrency review in the development review process, to ensure adequate capacity for new students.

The City of Temple Terrace is currently working with the School District on a countywide plan to develop a number of strategies to mitigate the effects of projected future residential growth for the entire City including the TTRA area. In Temple Terrace, efforts to mitigate the effects of expected increases in the number of residential units within the TTRA area may include setting aside lands for expansion of existing facilities or construction of new schools, and innovative techniques to be developed in partnership with the School Board to mitigate effects of residential growth.

5.6 Community Policing Initiatives

The City of Temple Terrace intends to incorporate community policing practices within the TTRA, and in addition has included public safety facility improvements among the capital improvement projects (listed in Section 8.3) in support of the goal to provide police presence 24 hours 7 days a week.

6.0 Private Enterprise

It is a goal of this Plan to both protect and improve current businesses, to encourage the development of local private sector enterprises, and to attract new businesses of differing scales into the TTRA area. Commercial redevelopment activities may result in some business displacement. Some of the retail uses in the shopping center are expected to transition over time. However, this is expected to be within the realm of normal turnover. Efforts will be made to accommodate the desires of the business owners. Business displacement and relocation will be addressed through the appraisal and negotiation process. Every effort will be made to find mutually beneficial solutions that are consistent with redevelopment objectives.

6.1 Commercial Corridors

The major commercial corridors located within the TTRA area are along N. 56th Street and E. Busch Boulevard. These two corridors primarily include a variety of commercial uses, including banks, dry cleaners, grocery and drug stores, restaurants, video rental, and auto repair. It is clear that both corridors can and should benefit from the CRA designation, in terms of both physical improvements to the roadways, for the benefit of pedestrians, bicyclists and motorists, and in terms of CRA assistance in business retention and commercial redevelopment.

6.2 Commercial Niches

The proposed land uses within the TTRA area are expected to provide a focal point for daily shopping needs, City services, entertainment, and cultural activities for nearby residents, and to provide the gathering place City residents have indicated they want.

To meet the needs of the wider surrounding community, some downtown areas offer concentrations of specific types of businesses. Marketing niche strategies that focus on attracting specific markets to specialty retail outlets have been employed successfully in other communities and could help provide a focus for business recruitment and expansion efforts in the TTRA area. Because a niche requires greater specialization of downtown's retail space, it can help the downtown become more competitive with existing retail centers.

Focusing on a strength identified during the process of updating the City's Comprehensive Plan during 2005, Temple Terrace could choose to draw on the multi-cultural aspects of the university community and create a business atmosphere which attracts entrepreneurs from other nations. There are already a number of businesses in the greater Temple Terrace which offer food and merchandise from other lands. In addition, Busch Gardens' African and Asian themed attractions are publicized worldwide. There may be an opportunity to expand entertainment and specialty goods retailing with an international theme. This strategy would support the reuse of small retail spaces to a greater extent than the redevelopment of large-scale retail uses.

7.0 Proposed Plan

7.1 Philosophy

A Community Redevelopment Area Plan must be based upon the requirements of law, as contained in Chapter 163, Part III, Florida Statutes. The objectives of the Plan are to eliminate the conditions of blight identified in the TTRA area and replace them with a new downtown for the City, achieve an economically sustainable level of redevelopment, and provide a safe and pleasant living environment for area residents, visitors, and business community. Assisting an urban neighborhood in achieving such a condition of health will also create new jobs and stimulate growth of the TTRA area's tax base.

This Plan will focus on leveraging the positive impact of key public sector initiatives, improving the transportation system, replacing infrastructure and improving the quality of life for current TTRA area residents, property owners, businesses, and visitors. It is intended to provide a facilitative framework for the marketplace, and to allow flexibility within the TTRA area to respond to changing market conditions and priorities.

A key concept in this Plan is to protect and improve the community assets in the TTRA area, while encouraging new higher-density mixed-use residential, office, and commercial development. On May 15, 2007, the City Council approved a revised framework for redevelopment of the downtown.

CITY COUNCIL APPROVED REDEVELOPMENT FRAMEWORK – 4/20/07

1. Medium density with no buildings over six stories in the redevelopment area.
2. Infrastructure financing: combination of developer payments, grants to City, and Federal/State appropriations to City of Temple Terrace.
3. Park and green space financing: combination of developer payments, grants to City, and Federal/State appropriations to City of Temple Terrace.
4. Phasing: two large phases over six years.
5. N. 56th St. to remain 4 lanes, to be landscaped with traffic islands, and traffic calmed: City financing through grants and appropriations.
6. Design and function: Mediterranean and New Urbanism, per Torti Gallas guidelines.
7. Regions Bank – accelerated relocation.
8. City Council may consider appointing an oversight committee and establish project review standards; Council to select community representatives.
9. Adopt Torti Gallas guidelines with transect modifications and small parcel treatment.
10. City to provide Clerk of the Works on redevelopment construction site.
11. Develop retail post office on redevelopment site; mail sorting to move to alternate site.
12. Reserve space for City Hall in Phase 2 of project; feasibility study to be conducted prior to Phase 2 beginning.
13. Community Arts Center to remain on site.
14. Keep retention pond in Phase 2 property; City to beautify.
15. Put police substation in redevelopment, unless City purchases other land in TTRA area and builds new police station. If police station is built off-site, it should be in Phase 1 of project.
16. Sidewalks should be primarily covered in the redevelopment area to provide shade and protection from rain.

17. Developer agrees in initial contract to buy all Phase 1 property with an option to purchase Phase 2, contingent on satisfactory Phase 1 performance and completion, and City Council approval.
18. Hillsborough River residences: Council prefers one/two story construction next to residential neighborhood and three/four story construction away from existing neighborhood. Council would consider residences as Phase 1 construction.
19. Items for discussion with developer: Parking garage financing (developer/City TIF financing preferred for public garage(s)); density; Burger King; Main Street construction.

The CRA Plan will be graphically represented by a generalized redevelopment plan, supported by a series of recommendations for public and private investment in the TTRA area. The Plan's purpose is to provide guidelines for actions intended to eliminate conditions of blight, including unsafe, inadequate or obsolete infrastructure, and to restore a perspective of resident, investor and developer confidence in the area. With the increased economic activity expected as a result of these efforts, the City can anticipate additional urban residents, new business development, jobs creation, and an enhanced ad valorem tax base in the TTRA area.

This Plan seeks to better integrate the redevelopment area into the larger urban fabric by improving pedestrian and vehicle connectivity to the downtown core and surrounding neighborhoods, introducing aesthetic elements that encourage the feeling of a sense of place, and providing financing for infrastructure as identified in the Finding of Necessity. The Plan seeks to develop solutions that will encourage a transportation and land use pattern more in tune with the downtown urban core. It seeks to integrate recent planning efforts that have taken place and those that will be coming to fruition starting in the upcoming months and continuing through the next few years.

Furthermore, the recommendations set forth here seek to improve the safety and aesthetic appearance of many of the current structures through the use of financial incentives and increased comprehensive code enforcement services.

Finally, the Temple Terrace Redevelopment Agency Plan encourages the assemblage of parcels by both private and public entities in order to create redevelopment opportunities.

7.2 Development Controls

Land use and zoning are governed by the City of Temple Terrace Comprehensive Plan and the City's Code of Ordinances. The 2001 CRA Plan recommended that a downtown redevelopment zoning district be drafted for inclusion in the City's Code to address such parameters as permitted uses, landscaping, signs, architectural controls, parking and setbacks. The intent of the recommendation is to further redevelopment plan implementation and to expedite approval of proposals that conform to the plan.

Separate but interrelated development controls have been adopted by the City to govern land use, set standards for design, and provide appropriate transportation systems to facilitate redevelopment of the TTRA area. These controls are the Downtown Mixed Use-25 land use designation that has been added to the adopted Temple Terrace Comprehensive Plan and Map; the Downtown Community Redevelopment Plan Overlay District incorporating design standards that have been developed and approved by City Council for application to new development and substantial redevelopment within the TTRA area; a separate set of amendments to the City's

(TCEA) which includes the entire TTRA area, to facilitate creation of a pedestrian-oriented downtown; and the intended use of Developer Agreements for implementation of public-private partnership redevelopment activities.

These controls will provide an integrated approach to redevelopment of a downtown that will be varied, accessible, efficient, and aesthetically pleasing. All new construction will comply with applicable local laws and ordinances. Proposals for new development and redevelopment will be reviewed by City administration and forwarded to the TTRA.

7.2.1 Temple Terrace Comprehensive Plan - Downtown Mixed Use-25

To insure orderly and compatible redevelopment of the TTRA area, a new future land use category was created and made part of the Temple Terrace adopted Comprehensive Plan. The Downtown Mixed Use-25 (DMU-25) Future Land Use designation encompasses the entire TTRA area and is intended to establish, define and facilitate the redevelopment of the City's traditional and historical downtown business district, and to insure such redevelopment is of an urban character. The DMU-25 land use category has a further purpose of implementing the goals, objectives and policies of the Future Land Use Element of the City's Comprehensive Plan to serve long-term community interests.

Permitted Uses and Zoning

The DMU-25 designation encourages a mix and range of uses that are typical of traditional urban core activities including commercial, retail, office, personal services, restaurants, entertainment, parks, institutional and public or semi-public uses, and medium and high density residential to support mixed uses in the TTRA area. In keeping with the effort to create a pedestrian friendly downtown, automobile oriented businesses such as gas stations and repair shops are discouraged, along with other activities such as manufacturing, warehousing or storage that do not fit in a compact, walkable downtown pattern.

Existing uses and zonings of properties within the TTRA area are incorporated into the DMU-25 until they are substantially redeveloped, at which time they would be required to rezone to site plan controlled planned development, subject to overlay or other additional controls that may be developed for the district, and must meet all requirements for new development within the TTRA area.

Intensity and Density

Intensive commercial and high-density residential uses are the most notable characteristics of downtown areas. A range of commercial floor area ratios between 3.5 to 4.5, will allow the highest ratios in the Urban Mixed-Use portion of the TTRA area on major pedestrian streets. In this area, parking garages can be integrated into site plans to maximize the amount of high-value land available for building. Residential densities averaging 25 units per acre are envisioned, with a projected 2,400 to 3,500 new units to be constructed.

Relationship to Adjacent Area and Street Network

Design standards for new construction within the TTRA area (discussed in detail below) were developed to implement the overall goals of the DMU-25 land use designation. These standards will be applied during development of site plans for new projects, and will address function, appearance, compatibility and connectivity of new development with the surrounding community.

7.2.2 Downtown Community Redevelopment Plan Overlay District

The City adopted this amendment to Chapter 29 of the Land Development Code as a set of detailed downtown design and development standards applicable to rezonings, new development and substantial redevelopment within any portion of the TTRA area, to implement the goals of the DMU-25 Comprehensive Plan designation in keeping with the City's master development plan and vision for a recreated downtown.

The guidelines apply maximum building heights, proportion of façade having continuous street frontage, and percentage of ground floor façade oriented to retail pedestrian traffic, all of which vary depending upon location of the project within a hierarchy of street types.

Designers of new projects may select from three building styles; Spanish Renaissance Revival, Mediterranean Revival, or St. Augustine Spanish Colonial, with massing, façade and roof materials, windows, entrance features, and trims appropriate to each building style. Finishes and choice of colors from the approved palette, together with traditional lighting fixtures and landscaping are required of all developments throughout the TTRA area.

To create a functional and attractive look that is consistent throughout the TTRA area, rules regarding signs, driveway widths, access, parking (including a maximum limit on the number of spaces that can be provided according to the type of use), pavement finishes, and location of drainage facilities and accessory structures will apply to all commercial and mixed-use projects within the TTRA area.

The City is anticipating the creation of a form-based land development code which will stipulate individual design treatments by lot and block for the TTRA area. The code work is anticipated to begin in 2007 or 2008.

7.2.3 Transportation Concurrency Exception Area (TCEA)

The TCEA, in lieu of transportation concurrency restrictions, is applicable to the entire TTRA area and encompasses a range of transportation options for improved accessibility and mobility to facilitate urban infill and redevelopment. Inefficient and inappropriate building patterns such as strip development with multiple access points shall be discouraged. Development features such as shared parking, cross-access, provision of on-street parking and allowances for van and car pool parking will be employed on site plans for new development to avoid the need to provide parking spaces in excess of a defined maximum number of spaces according to the type of use.

Design of new streets and improvement of existing streets within the TTRA area will employ traffic calming features such as chicanes (jogs or curves), reduced lane widths and corner radii, lowered speed limits, curb extensions, T-intersections, roundabouts, gateways, and speed tables.

The TCEA incorporates further strategies to reduce the number of vehicular trips to the TTRA area, including enhanced public transit service, and roadway improvements and streetscaping that will encourage pedestrian movement, such as walking and biking facilities provided within a road system that features a high level of pedestrian mobility and connectivity. Traffic calming measures will be employed to reduce traffic speeds and streetscaping details such as lighting, benches, bus shelters, and bike facilities will encourage walking and biking.

7.2.4 Diversity of Property Ownership, Outdated Building Density Patterns and Sub-Standard Lot Sizes

The City may consider non-*eminent domain* related assistance to private property owners in facilitating property assemblage for redevelopment. Such assistance may include but is not limited to: providing marketing expertise, assisting in the acquisition of unused easements and "left-over" and unusable parcels, parking agreements, provision of public parking facilities, business recruitment and provision of public amenities attractive to larger scale developers. The CRA designation will greatly enhance the City's ability to provide technical, legal and financial assistance.

7.3 Land Uses and Street Layout

In order to determine the appropriate land uses and street layout for the TTRA area, a series of analysis maps were prepared. First, Map 7.1, Context Analysis, shows the proposed CRA in relation to the surrounding jurisdictions that are physically interrelated, and thus are affected by the viability of the CRA. Its successful redevelopment will add value to these surrounding neighborhoods, and create a synergy from which all will benefit.

Further analysis identifies the location of all existing community assets, including places of worship, parks and recreational facilities, schools, libraries, fire stations, and other city properties (See Map 7.2, Community Assets). The Plan's focus in this regard will be to preserve and improve these assets. Finally, a map of the existing road network was provided to understand where the street grid lacks connectivity, and to evaluate future connections. The limited street network has created large tracts of land of a disconnected, suburban nature, and lacks an urban pattern of walkable scale. Further, the limited roadway network is burdened with an unfair share of the vehicular traffic, and therefore has less capacity for pedestrian and bicycle amenities (See Map 7.3, Existing Street Network).

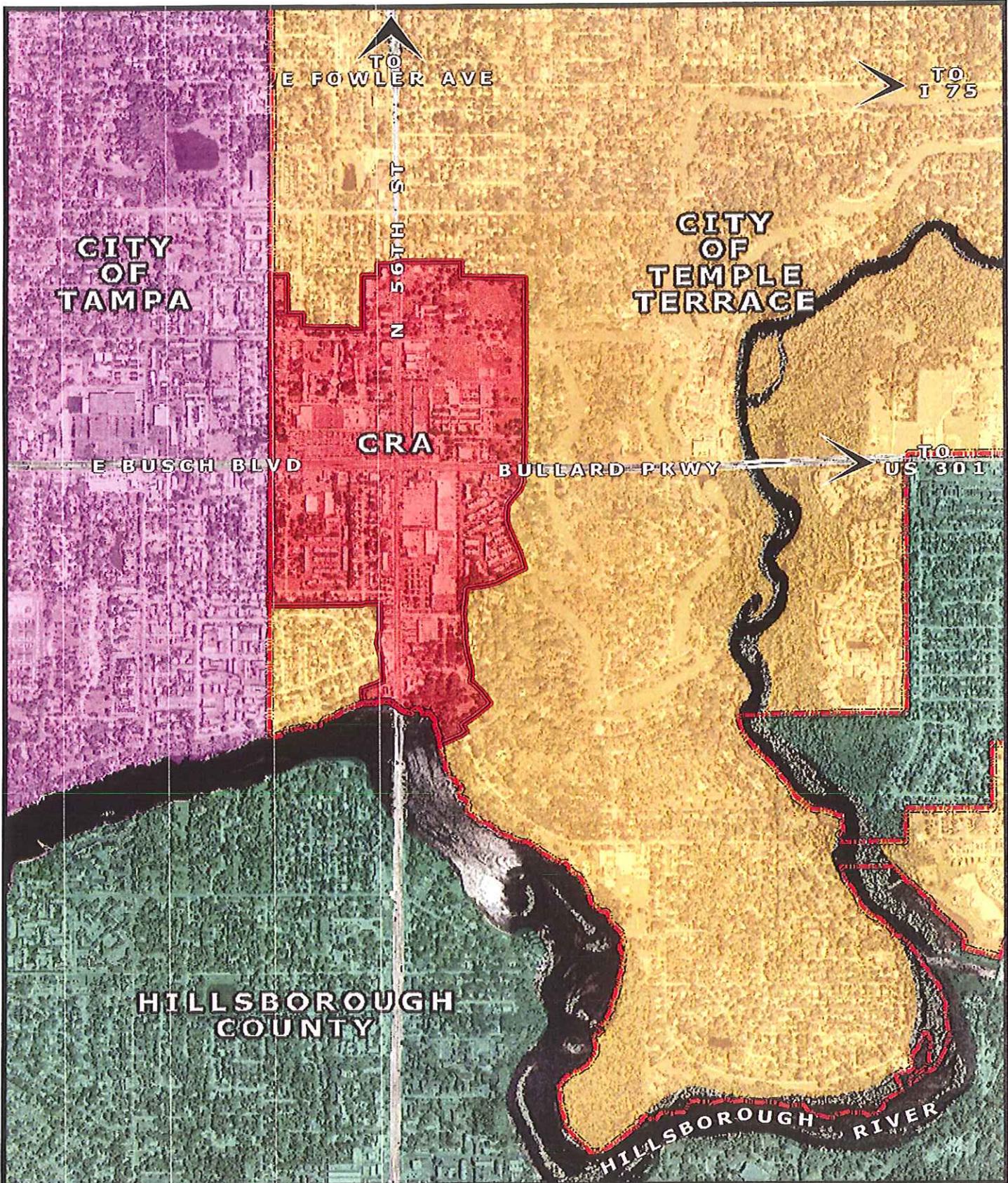
The urban pattern will be re-established upon redevelopment by reconnecting the street network. Adding new access points will help to recreate the street grid that once existed in the TTRA area. As shown on the Redevelopment Plan diagram Map 7.4, there are several new access points proposed. Of particular importance are the new connections for internal connectivity.

Other aesthetic improvements could include the usage of decorative paving to denote pedestrian crosswalks and decorative street lighting. New gateway elements, street signs, and wayfinding features should welcome visitors into the TTRA area and in time help in the development of a new sense of entry and place.

The Redevelopment Plan Map 7.4 proposes three general land use groups for the redevelopment area: Urban Mixed-Use, Residential Mixed-Use, and Public/Quasi-Public/Institutional/Open Space.

7.3.1 Urban Mixed Use

The Urban Mixed-Use portion of the TTRA area can be considered the heart of the redevelopment plan, encompassing its center and new downtown core, including the central intersection at E. Busch Boulevard and N. 56th Street. It spans both sides of E. Busch Boulevard/Bullard Parkway from the western boundary of the TTRA area along the west side of Overlook Drive eastward to Broadway, and both sides of N. 56th Street from the south gateway



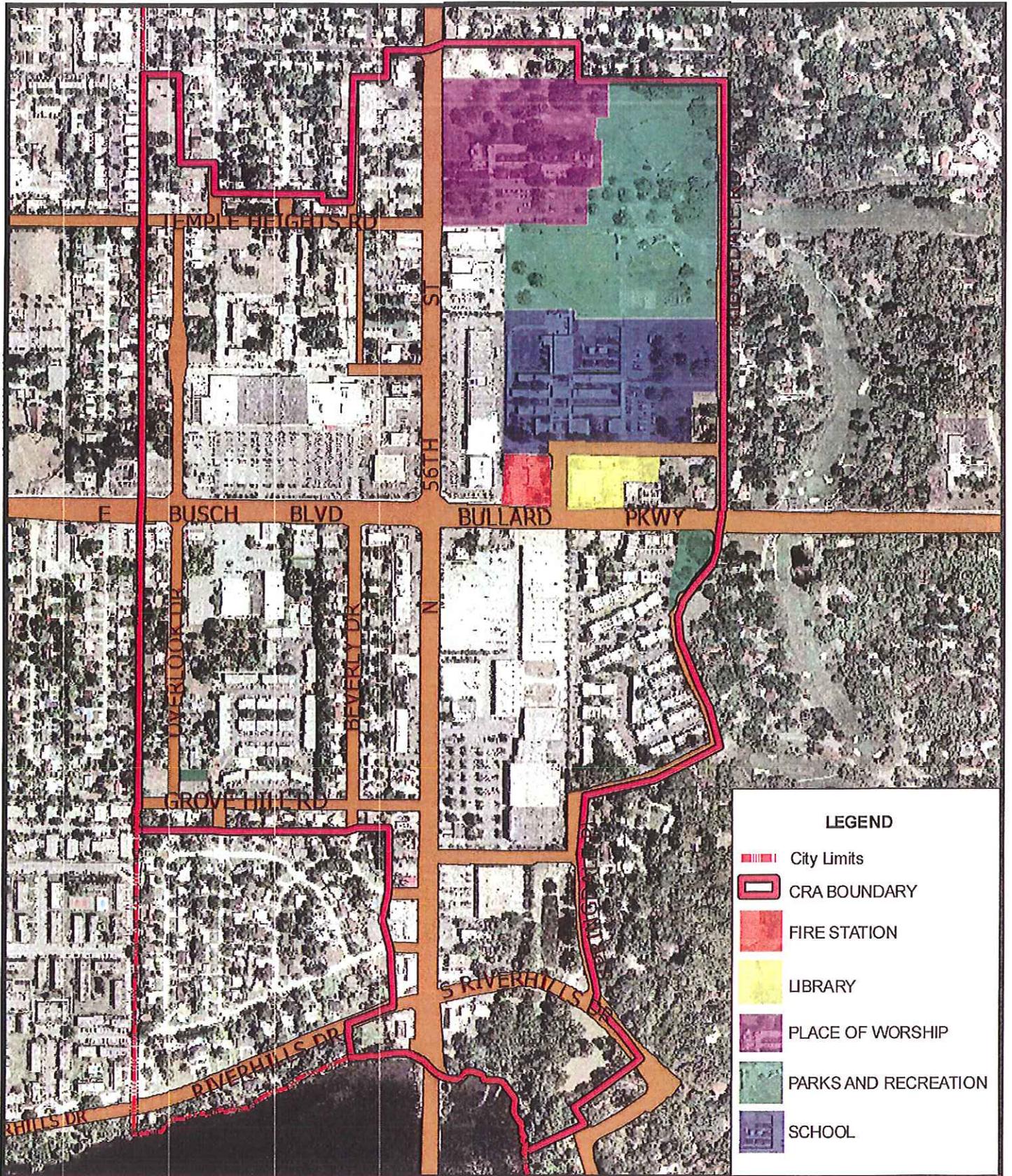
DOWNTOWN CRA PLAN UPDATE

MAP 7.1 CONTEXT ANALYSIS

500 250 0 500 1,000 Feet



GIS
Community Services
November 2006



LEGEND

-  City Limits
-  CRA BOUNDARY
-  FIRE STATION
-  LIBRARY
-  PLACE OF WORSHIP
-  PARKS AND RECREATION
-  SCHOOL

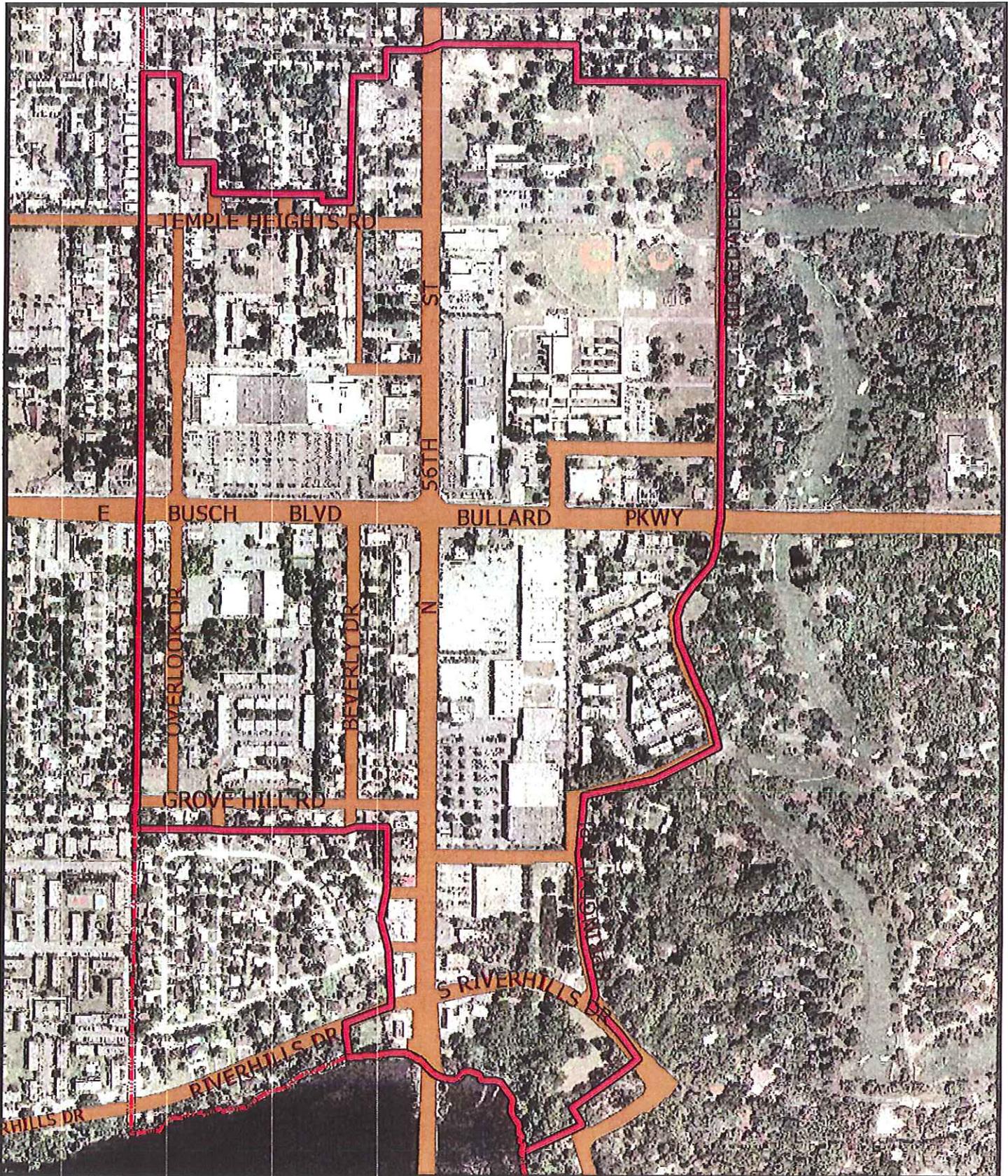


DOWNTOWN CRA PLAN UPDATE

MAP 7.2 COMMUNITY ASSETS



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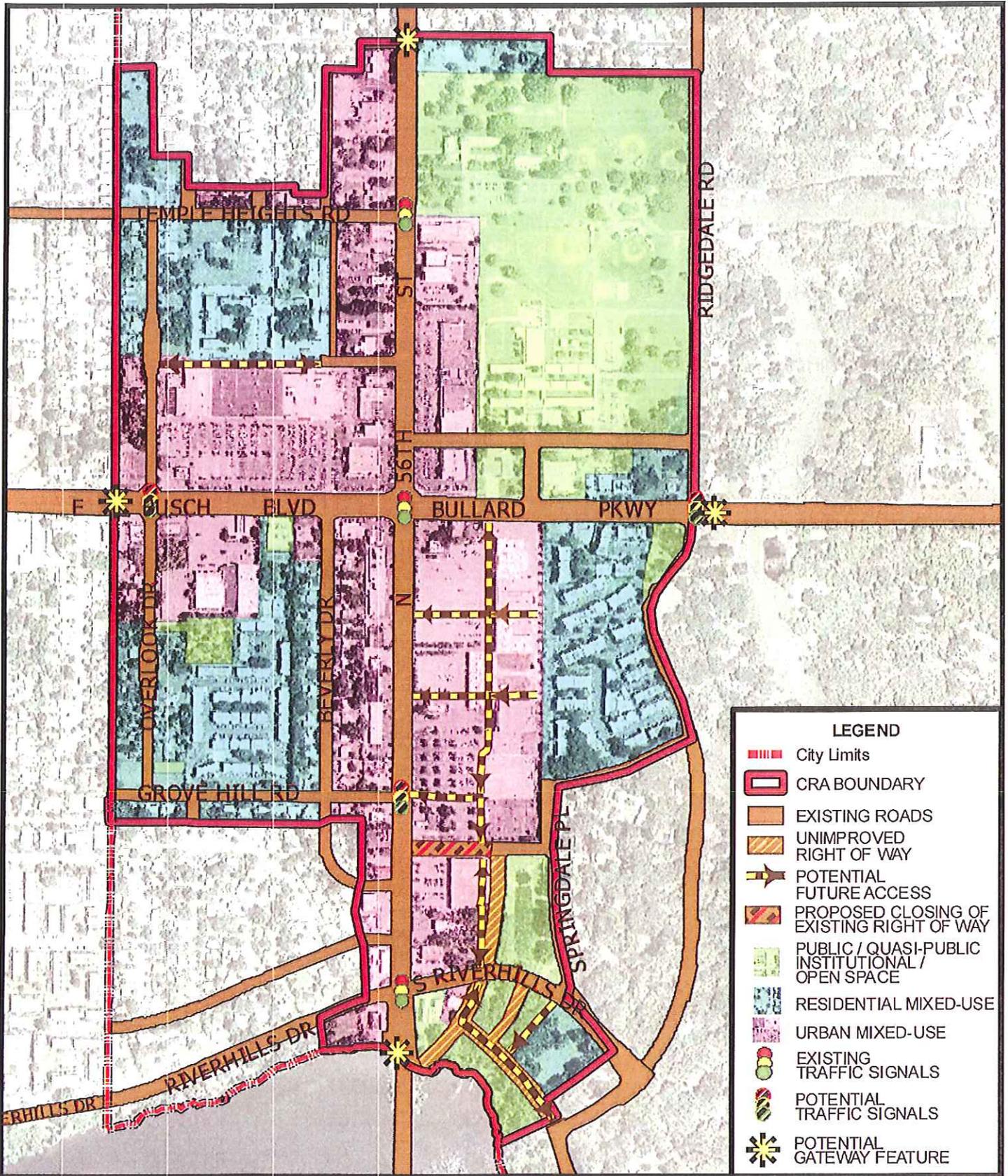
DOWNTOWN CRA PLAN UPDATE

MAP 7.3 EXISTING STREET NETWORK

500 250 0 500 1,000 Feet



GIS
Community Services
November 2006



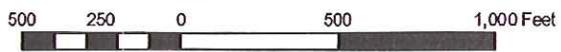
LEGEND

- City Limits
- CRA BOUNDARY
- EXISTING ROADS
- UNIMPROVED RIGHT OF WAY
- POTENTIAL FUTURE ACCESS
- PROPOSED CLOSING OF EXISTING RIGHT OF WAY
- PUBLIC / QUASI-PUBLIC INSTITUTIONAL / OPEN SPACE
- RESIDENTIAL MIXED-USE
- URBAN MIXED-USE
- EXISTING TRAFFIC SIGNALS
- POTENTIAL TRAFFIC SIGNALS
- POTENTIAL GATEWAY FEATURE



DOWNTOWN CRA PLAN UPDATE

MAP 7.4 REDEVELOPMENT PLAN



GIS
Community Services
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at the Hillsborough River northward to 98th Avenue, including a portion of the adjoining north side of Temple Heights Road.

Allowed uses in the Urban Mixed-Use area offer a full array of uses typical of the downtown core – retail, commercial, high-density residential, recreational, civic, cultural and institutional. The TTRA downtown redevelopment guidelines specify highly pedestrian-oriented retail ground floor uses, high commercial floor area ratios, high residential densities, and building heights up to 6 stories for this portion of the TTRA area, in keeping with its function as the central downtown district.

At the outer edges of the Urban Mixed-Use area, commercial intensities, residential densities and building heights will be generally less than those in the core area. These areas will function as transition areas between the highly pedestrian-oriented downtown core and the residential, public and institutional uses and open space located at the outer edges of the TTRA area. Site development plans will be reviewed for compatibility with adjacent neighborhoods.

The focal point of redevelopment in the TTRA area are the Terrace Plaza and Sweetbay Shopping Centers located in the southeast quadrant of the Bullard Parkway and N. 56th Street intersection. The Sweetbay grocery store and other plaza buildings will be retained to serve as a base for new development. Underutilized space on the north end of the Sweetbay Plaza could be developed as an Arts Center, and the post office could remain in new or renovated spaces to form a nucleus of cultural activities and public services in the redeveloped urban center. Space could be reserved in the Phase 2 portion of the project for additional future public uses. Combined existing and new development is expected to increase the existing square footage to over 200,000 square feet.

The vacant Chevron station at the northwest corner of N. 56th Street and Chicago Avenue in the southeast quadrant has been demolished to provide prime roadway frontage for retail development, a parkette/gathering space, or additional retail. Demolition of the less functional portions of the existing Sweetbay Plaza will allow for construction of parking and other infrastructure in support of the new development.

Also in the southeast quadrant, a possible redevelopment scenario for the area southeast of the intersection of Chicago Avenue and N. 56th Street, bounded on the south by Riverhills Drive and public open space, and on the east by a proposed new road and existing drainage facility, is to relocate the postal service distribution facility out of the area to facilitate development of up to 60,000 square feet of new office space with ground floor retail or six-story residential development. The site, as the south gateway to the CRA, could provide a relocation opportunity for one of the banks within the downtown core area, as well as provide shopping amenity and river views from the upper floors for residents, office workers and visitors.

In the southwest quadrant, existing retail, restaurants, offices and residences have potential for redevelopment as a mixed-use area with specialty retail, dining and entertainment venues fronting N. 56th Street and E. Busch Boulevard. Existing and redeveloped offices and residences are expected to continue to predominate on Beverly Drive. Parking to support existing retail on N. 56th Street may be part of the Beverly Drive redevelopment effort. The bowling alley site has substantial promise for mixed-use development.

In the northwest quadrant, an existing neighborhood shopping center containing a K-Mart and a Publix grocery store front the north side of E. Busch Boulevard west of N. 56th Street, with a

Walgreens drug store forming an effective anchor at the corner of the E. Busch Boulevard and N. 56th Street intersection. These stores can be expected to provide supporting retail for the new development on N. 56th Street, and the site has potential for redevelopment for a combination of mixed uses including office, retail and residential.

To the north of Walgreens on N. 56th Street, existing office development, including several financial institutions, has potential for redevelopment as a more intensive office and financial center that would support and reinforce the downtown core business district.

Also in the northwest quadrant, an area at the northern edge of the TTRA area fronting the west side of N. 56th Street northward from Temple Heights Road to 98th Avenue, and extending from N. 56th Street westward along the north side of Temple Heights Road east of Overlook Drive, a vacant funeral home and a vacant former post office building have potential for redevelopment for more intensive office or mixed uses. The south parking lot of the former post office site has been approved for development of 9,000 square feet of office space according to a site plan approved under the downtown redevelopment and architectural guidelines. The project is currently under construction and expected to be completed in 2007.

In the northwest quadrant near the western edge of the TTRA area spanning both sides of E. Busch Boulevard in the vicinity of Overlook Drive, the existing residences on Overlook Drive and the retail located on the south side of E. Busch Boulevard have potential for redevelopment for a combination of office, retail, indoor recreation and residential uses at higher floor area ratios and residential densities than exist in the current development pattern.

In the northeast quadrant, the neighborhood shopping center fronting the east side of N. 56th Street and anchored by the CVS drug store at the northeast corner of N. 56th Street and Bullard Parkway is expected to continue to contribute retail shopping support to the downtown core area. Planned building renovations and improvements to access, parking, open space and landscaping may assist the center's transition.

The main roads serving the TTRA area are classified as Retail Pedestrian in the redevelopment plan street hierarchy, and they will be designed for the highest level of pedestrian access and amenities, landscaping and lighting. Up to five new roadways, including a new Main Street, are planned to provide improved vehicular and pedestrian access to the redeveloped plaza, improved circulation within the TTRA area, and provide new connections to the surrounding road network. Streets classified as Primary and Secondary will serve less intensive portions of the TTRA Urban Mixed-Use area. Landscaped public spaces and pedestrian walkways will connect to other pedestrian ways leading southward to public amenities planned for the Hillsborough River and gateway area.

7.3.2 Residential Mixed-Use

The Residential Mixed-Use area is the least intensive of the mixed-use designations and is found in five locations mainly along the periphery of the TTRA area, and described as follows.

- 1) In the southeast quadrant, the Doral Oaks Apartment complex on the south side of Bullard Parkway, together with a group of offices across Bullard Parkway to the north, form the eastern edge of the redevelopment area. Existing development within the area has potential for renovation to make it more compatible with the downtown architectural guidelines, or redevelopment for projects that might combine

medium density residential such as condos or townhomes, and office and neighborhood supporting retail.

- 2) In the southwest quadrant, existing single family and multi-family developments including the Brentwood Apartments within the area bounded by both sides of Grove Hill Road, both sides of Overlook Drive, and the west side of Beverly Drive, have potential for redevelopment at medium office and commercial intensities and residential developments such as condos and townhomes.
- 3) In the northwest quadrant, the existing single family and multi-family developments within the area of Temple Heights Road on the north and extending from the west side of Overlook Drive eastward to a potential extension of Beverly Drive have potential for redevelopment as combined residential and office projects at commercial intensities and residential densities in keeping with their location abutting the downtown core. The Bella Terraza Townhomes project is currently under construction within this area.
- 4) In the northeast quadrant on the east side of N. 56th Street at the north end of the redevelopment area, an undeveloped parcel under City ownership has the potential for development as single family lots, and is a possible area for relocation of residential property owners from redeveloped sites who wish to continue to reside in the area.
- 5) In the southeast quadrant, a currently undeveloped parcel near the southeast tip of the redevelopment area south of the current terminus of Springdale Place has potential for an upscale medium-density residential development with river and downtown views.

7.3.3 Public/Quasi-Public/Institutional/Open Space

The Public/Quasi-Public/Institutional/Open Space areas are mainly two areas at the northeast and the southeast corners of the TTRA area, respectively, with three small sites ranging across the redevelopment area south of E. Busch Boulevard/Bullard Parkway.

- 1) In the northeast quadrant, an area comprising most of the northeast portion of the redevelopment area bounded on the north by homes facing Linda Avenue, on the east by Ridgedale Road, Bullard Parkway on the south and N. 56th Street and a portion of the Urban Mixed-Use area on the west contains a number of long-standing public and semi-public uses. The major uses within the area are a church, an elementary school, and a City-owned sports complex, and the City fire station and library together fronting the north side of Bullard Parkway. These uses will support the existing and new residential and commercial uses within the redevelopment area.
- 2) In the southeast quadrant, the southeastern edge of the TTRA area extending southward from Chicago Avenue west of Springdale Place and extending from there southward to the Hillsborough River, contains existing drainage facilities that are planned for expansion and addition of park and open space. Planned pedestrian walkways and amenities will connect leisure-oriented riverfront activities to the downtown core within walking distance to the north.

- 3) In the southeast quadrant, an existing BP gas station fronting the east side of N. 56th Street at the southern City gateway should be renovated as an interim measure and ultimately redeveloped in keeping with its river front location for uses such as dining, open space as part of the adjacent park expansion or recreational facilities supporting downtown core activities. The BP station, along with two adjacent parcels owned by Shell Oil, are contaminated and in the State of Florida clean-up program. If the City were to purchase these parcels and clean up the sites, a reimbursement request has substantial opportunity to be funded for all purchase and mitigation costs through the Florida Forever Program.
- 4) On four different sites south of E. Busch Boulevard are a cemetery and Overlook Park on the east side of Overlook Drive, an electrical utilities facility on the south side of E. Busch Boulevard west of Beverly Drive, and public open space adjacent to the golf course on the south side of Bullard Parkway at Sunnyside Road at the eastern edge of the TTRA area.

7.4 Infrastructure-Related Needs

The general condition of deteriorating and aging structures is remedied slowly, using a variety of approaches to either renovate and restore existing structures or demolish buildings and redevelop property. Such tools could include but is not limited to concerted code enforcement, facade improvement loans and grants, assistance in packaging Small Business Administration (SBA) and other special renovation loan programs, facilitating property assembly and redevelopment, sale of City property, special tax deferral programs, City general revenue funds, general obligation bonds, and other financial and technical inducements.

7.4.1 Storm Drainage

The property east of N. 56th Street typically drains to the east to the City's major collection system at the rear of Terrace Plaza, which is conveyed to a recently installed continuous deflective separation (CDS) unit and detention pond south of Chicago Avenue prior to discharging into the Hillsborough River. The CDS unit was constructed in 2001 and provides water quality treatment for the southeast quadrant of the TTRA area.

Drainage of property west of N. 56th Street is provided by a major collection system along N. 56th Street which flows south and discharges into the Hillsborough River south of Riverhills Drive. This system is owned and maintained by the Florida Department of Transportation (FDOT). A CDS unit was also installed south of Riverhills Drive to provide water quality treatment for the area west of N. 56th Street. Most of this area already has a high percentage of impervious cover, and there is sufficient capacity in the system. Redevelopment will be subject to meeting FDOT's connection requirements.

Preliminary discussions with SWFWMD staff indicate that stormwater attenuation or additional treatment will not be required in the southeast quadrant of the TTRA area. A formal policy for the redevelopment area will be finalized with SWFWMD at a later date.

7.4.2 Utilities

Public water and sanitary sewer service is available to all quadrants of the TTRA area. There do not appear to be any major constraints in providing service to the area. The City's Potable

Water Master Plan was updated in 2005 and verified that the existing transmission/distribution system will have sufficient capacity for the growth resulting from the future redevelopment.

The sanitary sewer system was determined to have capacity to serve the TTRA area. The City currently has under design the replacement of the major pump station including the force main under the Hillsborough River which serves the redevelopment area. The design will take into consideration the growth expected from the new development.

In several areas, relocation of existing water and/or sanitary sewer lines may be required due to the proposed location of new buildings or streets, and the realignment of existing streets. This provides an opportunity to replace aging pipelines concurrently with roadway construction.

A new electrical power sub-station and transmission lines may be needed to supply the additional load to serve the anticipated redevelopment. Under the redevelopment plan goals to improve the quality of public services as well as the physical appearance of the TTRA area, the City will pursue every potential avenue of funding to place all Tampa Electric Company (TECO) and other utility lines underground. The required upgrading of service on individual properties will be privately or publicly funded.

In conjunction with TECO, the City should carefully inventory and evaluate the TTRA area's street lighting. There are many scattered pockets throughout the TTRA area that are inadequately lit at night, for both pedestrians and motorists. It appears to be a contributing factor to the incidence of illegal dumping and other crime in those areas, as well. A comprehensive program should be undertaken to provide adequate and appropriate street lighting throughout the TTRA area.

Consideration should also be given to encourage the development of Wireless Internet (WIFI) to the TTRA area, where medium to high-density residential development is expected to attract college students and other residents who are likely to create a market for such services.

7.4.3 Streetscape

In addition to this basic improvement, traffic calming devices, clear crosswalks, streetscape, and other pedestrian amenities and safety features are needed on N. 56th Street, E. Busch Boulevard/Bullard Parkway, and throughout the TTRA area. A consultant landscape architect has prepared and presented to City Council a streetscaping plan that will provide specific recommendations for improvements selected and coordinated to create an ambiance uniquely appropriate to Temple Terrace, including tree planting and landscaping of roadways, paving pattern and materials, pedestrian crosswalks, decorative street lighting, street furniture such as benches and trash receptacles, and decorative elements such as banners.

A comprehensive, creative and flexible plan should be designed to provide street resurfacing, new sidewalks and on-street parking, new public streetscape and landscape, public art and transit stops. Every street in the TTRA area should be evaluated for physical current condition and the need for resurfacing. Specific suggestions for managing current traffic loads, circulation and other issues are discussed under Transportation (Section 7.4.6).

7.4.4 Gateways and Signage

Major gateways should be identified and recognized with special signage and public art. E. Busch Boulevard/Bullard Parkway and N. 56th Street should be evaluated for new pedestrian-

friendly, urban design improvements. Important gateways should integrate landscaping and monument features that will help define the neighborhood's identity. It is expected that small parcels of land may be acquired at intersections where feasible to allow for sufficient space to create appropriate and aesthetically pleasing gateway features that could include a small park.

Signage within the TTRA area should be unique to its downtown character. Wayfinding signage should be implemented throughout the TTRA area by use of decorative signs that greet and guide travelers to points of interest. Iconic graphic emblems, for example the temple orange, could be developed as "brand" images that can be utilized in marketing the TTRA area.

7.4.5 Parking

The City will evaluate the feasibility of providing public parking facilities in the TTRA area to enhance the redevelopment potential of private property. Such facilities could include surface parking lots, structured parking facilities and additional on-street parking. The CRA designation will enhance the City's ability to underwrite the cost of such facilities. Development strategies to control the proportion of redeveloped land to be dedicated to parking include maximum limits to the number of parking spaces provided, shared parking, and roadway design to accommodate on-street parking.

7.4.6 Transportation

Strategies for calming traffic provided for in the TCEA (discussed in detail in 7.2.3) will require design of new streets and improvement of existing streets within the TTRA area to create a finer grained grid network that includes features such as chicanes (jogs or curves), reduced lane widths and corner radii, lowered speed limits, reduced number of curb cuts, curb extensions, T-intersections, roundabouts, gateways, and speed tables. A substantial expansion to the sidewalk system, provision of bike lanes and facilities with connections to transit stops, and streetscaping enhancements such as pedestrian crosswalks at the intersection of N. 56th Street and E. Busch Boulevard and other strategic locations, lighting, benches, bus shelters and other pedestrian amenities will also be needed to encourage walking and biking. A pedestrian bridge linking the Library and Temple Terrace Elementary School to the southeast quadrant should be considered.

The City has commissioned a transportation study with the TTRA area. Traffic islands proposed on N. 56th Street south of the Busch Boulevard/Bullard Parkway/N. 56th Street interchange will impact access to the businesses on the west side of 56th Street. Additionally, new signalization is proposed for the following intersections: Overlook Drive/Busch Boulevard, Grove Hill Drive/Chicago Avenue, and Ridgedale Road/Sunnyside Road/Bullard Parkway. Other significant recommendations are made in the report and all should be considered for implementation.

7.4.7 Transit

Current and future residents of the TTRA area will need a dependable transit system that includes improvements such as new circulator buses and trolleys, later hours of bus service, and increased frequency of buses on existing routes. In addition, more frequent service on some bus routes, especially on Sundays, and diversifying routes serving the area would eliminate some of the need to transfer by providing more direct service. HART should be encouraged to evaluate the system to determine the best way to extend dependable and reliable service in the TTRA area.

In addition, improvements to transit stops are needed to ensure that each is provided a pull-off area out of the travel lane, and a well-shaded seating area and reasonable protection from inclement weather. Shelters and benches should be placed away from the curbs of busy roadways, and all should have complete sidewalk access. The City is planning initiatives with the University of South Florida to link students and employees to the TTRA area via shuttle buses and pedestrian and bicycle facilities.

7.4.8 Open Space

Throughout the life of the TRRA Plan, TIF and other readily available funds may be utilized to improve existing recreational uses, and is expected to fund additional park and public open space facilities, creating attractive public assets that will be appreciated and utilized by residents from the TTRA area and linked to the public open space systems throughout the City of Temple Terrace. Provision of such amenities will enhance the attractiveness of the area to developers and residents. Financing for such improvements will be facilitated by the CRA designation.

7.4.9 Schools

It is possible that additional school facilities may be needed if the TTRA area meets its development potential. Increased residential use may add to the student population. Concurrency requirements identify multiple techniques to accommodate growth, including impact fees, school choice assignments, school facility additions and new school buildings. The City should work closely with the School District in monitoring the transformation of the neighborhood and increased student demand to ensure that adequate facilities are in place as the population increases.

7.5 Program-Related Needs

The establishment of the CRA designation for the District greatly enhances the City's ability to undertake the activities necessary to stimulate redevelopment. The City will evaluate a wide variety of programs and projects in the TTRA area, to remedy the identified blighted conditions. Examples of such initiatives are discussed in Section 7.6, Redevelopment Tools.

7.5.1 Crime Prevention

A variety of preventative measures will be employed by the City to ensure the safety of residents and visitors in the TTRA area and protect property, including crime prevention techniques of community policing programs, based upon positive relationships built over time between law enforcement personnel and community members. In addition, design concepts of the Crime Prevention Through Environmental Design (CPTED) program should be incorporated into redevelopment plans to take advantage of natural surveillance, access control and territorial reinforcement to reduce the incidence of crime and enhance the quality of life in the TTRA area.

7.5.2 Neighborhood Association

In the event that the Downtown TTRA area residents are not adequately represented by existing neighborhood associations, a new neighborhood association should be formed. In addition, businesses in the TTRA area would benefit from establishment of a merchant's association.

7.5.3 Care of Property

The City will build upon existing programs such as the Housing Rehabilitation Program, which provides low interest loans to residents who need help to bring their properties into compliance with City codes. Through an interlocal agreement with the County, the City administers loan funds to low-to-moderate income and elderly City homeowners (the loan does not have to be paid back if the owner lives in the home 10 years). The program is funded for approximately \$200,000 through 2006, (funding is renewable annually) and has assisted Temple Terrace residents with over \$1 million in rehabilitation funds since it began in 1995. All available programs providing assistance for housing rehabilitation should be utilized in the core residential area, including a concerted public education program. Priority should be considered for existing residents, particularly the elderly, who reside in the TTRA area.

7.5.4 Marketing and Staffing

The CRA designation and workload associated redevelopment activities may require the City to create new staff positions to administer activities of the TTRA area, and provide operational funding (for example, the production of marketing materials). These activities would include coordination of public and private development interests, regular and concerted marketing and public relation/education activities, master planning for the provision of infrastructure, public amenities and public services, production of annual reports, and administration of a façade improvement program, that would encourage cosmetic improvements and addition of urban elements to facades such as awnings, new windows, doors, and signage.

7.6 Redevelopment Tools

The application of a Community Redevelopment Area designation makes a number of redevelopment tools available to the Temple Terrace Redevelopment Agency Board (Agency), in this case, the Temple Terrace City Council. The following is a brief description of those redevelopment mechanisms:

- **Property Acquisition** – the current law authorizes the Agency to acquire real property through purchase, condemnation, or other lawful means, to assist or facilitate the redevelopment process.
- **Property Rehabilitation** – the Agency may rehabilitate, or require to be rehabilitated as part of a lease or sale, any property within the TTRA area.
- **Structure Relocation** – if a property is deemed to be worthy of rehabilitation and is in the path of a redevelopment project, the Agency may authorize the funds necessary to move it to a new location.
- **Cooperation with other public agencies** – the Agency will cooperate and coordinate with other public entities in the implementation of this Plan, in order to optimize the value of the community's investment in the TTRA area.
- **Property Management** – the Agency may, at its discretion, enter into agreements to ensure the preservation, maintenance and/or operation of real property within the TTRA area.

- **Demolition and Clearance** – the Agency may authorize the demolition, removal or clearance of buildings, structures and other improvements on real property it has purchased within the TTRA area, to aid its redevelopment.
- **Preparation of Building and Development sites** – the Agency may undertake building and/or site preparation on any real property within the TTRA area to assist in the redevelopment process, including environmental remediation.
- **Public Improvements and Infrastructure** – the Agency shall advise the City on the timing and location of public improvements, including surface improvements and subsurface infrastructure, to be accomplished within the TTRA area. The recommendations shall be consistent with the adopted Comprehensive Plan.
- **Real Property Disposition and Development** – the Agency may sell, or otherwise dispose of, real property within the TTRA area, in accordance with the Community Redevelopment Area Plan. The property may be disposed of at Fair Market Value, rather than at the Appraised Value. The documents of conveyance will contain any conditions necessary to safeguard the goals of the CRA Plan.
- **Relocation Policy** – It shall be the policy of the Agency that any persons displaced as a result of redevelopment projects shall be provided services that will ensure they are not unduly inconvenienced by their relocation. In the implementation of this Plan, the Agency will provide fair and equitable treatment to any and all displaced persons; including full opportunity to occupy comparable replacement housing, with regard to cost, location and related aesthetic considerations. The Agency will attempt to minimize the disruptions caused by the relocation, provide maximum choice to those required to relocate, provide whatever ancillary relocation services may be required to those in need of special assistance, such as the elderly or physically challenged, and make a diligent effort to avoid any financial hardship to relocating persons, due to the relocation process. The Agency will observe the same guidelines with any businesses and/or non-profit organizations that may be required to relocate.

Relocation assistance will be provided through the City Department of Community Services. Such assistance will be provided in a fair, equitable, and thorough manner.

- **Public Private Partnerships** – These types of agreements are important strategies in which a City service or private business venture is funded and operated through a partnership of government and one or more private sector companies. The City may use tax revenue to provide capital for investment, with operations run jointly with the private sector or under contract.
- **Grant Programs** – The City will pursue funding through all available sources for specific needs within the TTRA area, such as roadway improvements, streetscaping, pedestrian and biking amenities and façade improvements.

8.0 Cost and Timing of Redevelopment

8.1 Development Scenarios

In the furtherance of this CRA Plan, the City has undertaken efforts to develop tax increment estimates in the absence of specific and discrete development activity. In order to provide a range of potential outputs, estimating the residential and commercial development opportunities and weighing these against anecdotal historic redevelopment activity as well as new development opportunities that may be provided by the changing landscape of the TTRA area, particularly the redevelopment to occur in the southeast quadrant of the CRA. Tax increment projections have been developed.

A number of large, high impact redevelopment opportunities are located in the Area, particularly in the SE quadrant of the TTRA area, though current disposition and other risks may delay actual redevelopment investment for some time. Infill redevelopment opportunities are present, but the current real estate redevelopment market activity is sporadic.

Table 5 summarizes the Tax Increment Finance revenue projections for the entire CRA.

**Table 5
Tax Increment Finance Revenue Projections, 2010-2034**

Year	Total Taxes Collected on Value of CRA District
2010	\$700,051
2011	\$728,053.84
2012	\$757,175.99
2013	\$787,463.03
2014	\$818,961.56
2015	\$851,720.02
2016	\$885,788.82
2017	\$921,220.37
2018	\$958,069.19
2019	\$996,391.95
2020	\$1,036,247.63
2021	\$1,077,697.54
2022	\$1,120,805.44
2023	\$1,165,637.66
2024	\$1,212,263.16
2025	\$1,260,753.69
2026	\$1,311,183.84
2027	\$1,363,631.19
2028	\$1,418,176.44
2029	\$1,474,903.50
2030	\$1,533,899.64
2031	\$1,595,255.62
2032	\$1,659,065.85
2033	\$1,725,428.48
2034	\$1,794,445.62

The CRA district received a 28% increased assessment in 2006, when the rest of the City increased only 15%. All three scenarios use a 4% increase in taxable values annually.

Properties within the CRA that are owned by the City are on the County tax rolls because they are generating revenue. All projects to be funded with tax increment revenues will be completed within the thirty-year life span of the Tax Increment Financing District, which ends in 2034.

Annual revenues can be applied to pay-as-you-go improvements, accumulated for large capital investments, or used as a pledge for bonds. In order to implement public redevelopment improvements, additional sources of funds will be needed.

8.2 Implementation and Safeguards

One of the clear intents of Chapter 163, Part III, Florida Statutes, is that the Plan be protected in terms of implementation and intent. It also makes clear that all private property developed in the TTRA area is to be developed consistent with this Plan. The following safeguards are offered to satisfy these requirements.

- Produce annual reports from City's staff to the Temple Terrace Redevelopment Agency concerning progress on the Plan. Tax increment revenues may be used to fund a full time staff to support the requirements of implementing and monitoring this Plan. It is also anticipated that for this Plan to be successful that marketing and research funds be allocated from the tax increment revenues to support the CRA.
- Require that the development of any private property within the TTRA area be developed in accordance with the provisions of this Plan, including any restrictions or covenants running with land sold or leased for private use as deemed necessary by the City. Furthermore, the City may utilize Developers Agreements, where applicable, to ensure that proposed developments are consistent with the goals of the Plan. The City may restrict purchasers or lessees from selling, leasing or otherwise transferring real property in the TTRA area, until the purchaser or lessee has completed the construction of any or all improvements as agreed upon with the City.
- Coordinate the City's five-year strategic planning process and any other mid-range planning processes in the City from succeeding administrations with the implementation of the Plan on a regular basis and suggest any inconsistencies or lapses, if any.

This Plan has a thirty-year List of Capital Improvements horizon (2001-2031), with the TIF extending from 2003-2033, during which substantial redevelopment of the TTRA area is anticipated to be completed. The Temple Terrace City Council may modify the Plan from time to time, in accordance with the appropriate provisions of local and state law. In accordance with statutes, the City will provide an annual report including complete financial statements to be presented at an advertised public meeting.

Facilitating the redevelopment process in the TTRA area will require additional public investment to stimulate private investment and remedy the causes of the blighted conditions identified in the September 10, 1999, Finding of Necessity for Redevelopment report.

The CRA designation will permit the City greater latitude in providing technical and financial assistance in identifying and solving those problems on both public and private property. Assistance could include joint application for grants and potential funding from the establishment of a Tax Increment Finance District, waivers of other development fees, and special loan programs.

The net result from these efforts by the City of Temple Terrace and other major public players, the property owners, residents and commercial tenants will be the steady and thoughtful redevelopment of the TTRA area. Redevelopment will dramatically increase property values and ad valorem tax base, and affect a replacement of the economic distress evident in the district today with economic health.

8.3 Capital Projects

In addition to the infrastructure improvements previously described in the Plan, the TTRA will utilize TIF funding to make various capital improvements throughout the area. A list of capital improvement projects by type is provided in Table 6. At this time, the specific costs of these projects are unknown, and final cost estimates will be in large part determined by the timing of construction, and associated hard and soft costs required to construct any given improvement.

**Table 6
List of Capital Improvements**

IMPROVEMENT TYPE	SPECIFIC IMPROVEMENTS
ROADWAYS	Medians, landscaping, crosswalks, traffic calming, on-street parking
STREETSCAPING	Landscaping, planters, paving, lighting, street furniture, gateway features, wayfinding features (information signage, kiosks, branding)
TRAFFIC SIGNALS	56 th Street at Grovehill Bullard Parkway and Ridgedale Overlook at Busch Boulevard
MULTI-MODAL IMPROVEMENTS	Bus stop facilities, bike routes and facilities, golf cart facilities, sidewalk system with connections to trails, curb cuts, ramps, crosswalks and above-grade pedestrian crossing
UTILITIES	Undergrounding of lines, water mains, WIFI services
BUSINESS IMPROVEMENTS	Façade improvements, sign program, and landscaping,
PUBLIC BUILDINGS	Public safety facilities/buildings, Arts Education Center, Parking garages
PUBLIC PLAZAS AND PARKS	Waterfront Park, Landscaping, fountains, equipment, furniture, art
OTHER EXPENDITURES	HUD Repayment (Year 1-20)

These recommendations are in accordance with the vision and goals of the CRA Plan, and associated documents, including the Comprehensive Plan, Vision documents, streetscape plans, the TCEA, the Downtown Design Guidelines, and others currently being developed such as a proposed form based zoning code for the CRA.

The City of Temple Terrace will retain the responsibility of insuring adequate public infrastructure and public safety, and a redevelopment environment conducive to achieving the Policies, Goals and Objectives for the TTRA area articulated within the Comprehensive Plan. The City will also retain the ability to enter into development agreements with private sector entities to assist in the achievement of those goals and objectives.

8.4 Funding Sources

The CRA will use multiple funding sources to carry out redevelopment objectives. Supported by City staff, the TTRA will seek private money as well as funding from state, federal and local government sources. There are a number of funding sources available to the Temple Terrace Redevelopment Agency and the City that can be used to supplement, rather than replace, anticipated tax increment revenues. Funding methods and sources that will be examined to implement the redevelopment projects include:

- 1) Tax increment financing. This is a power delegated by statute to the Temple Terrace Redevelopment Agency. This method captures the increase in tax revenue in the district and directs it to a special redevelopment trust fund. Revenues from the City of Temple Terrace and Hillsborough County will accrue to the trust fund. The fund captures all increases in value, whether from redevelopment or appreciation of existing property. Any decreases in value are also reflected in fund revenues. For example, the tax yield from a large development on one site can be offset by a reduction in value in an existing property.
- 2) Special taxing district. A special district can be formed for the purpose of levying a millage assessment within a defined area to pay for services and improvements.
- 3) Federal and state funds. Funding for redevelopment is an eligible expense of the Community Development Block Grant (CDBG) program, administered through Hillsborough County for the Federal Government. Urban infill grants were authorized by the State Legislature in 1999. Implementation grants will be made available on a 50-50 matching funds basis.
- 4) Federal transportation funds may be available through several grant programs for improvements to state roads. Enhancement of roads (landscaping, drainage, pedestrian paths) may be accomplished under the SAFETEA-LU (formerly ISTEA) program. Drainage improvements may be funded by the Southwest Florida Water Management District. Housing funds for new construction or renovation may be available through the State-administered SHIP and SAIL programs. Federal tax credits are also available to support construction of new rental housing.
- 5) Revenue bonds may be issued by public agencies (such as the City of the TTRA) to finance eligible public improvements. Utility improvements are commonly funded through revenue bonds. General Obligation bonds or Industrial Revenue bonds may also be an option.
- 6) Local funds have been used to support development of this plan. General funds are also the source for improvements which benefit the overall community, such as parks, government buildings and local streets.
- 7) Community Investment Tax funds received by the City since the 1996 half-cent sales tax was levied by Hillsborough County to be shared with the municipalities, including Temple Terrace, and the School District to finance capital and public facilities projects.

The Temple Terrace Redevelopment Agency and City of Temple Terrace will regularly evaluate potential projects and available funding sources.

Redevelopment is expected to occur over the life of this Plan, which will not exceed thirty (30) years as allowed by Florida Statutes. It is within the powers of the City of Temple Terrace to choose either pay-as-you-go financing or long term indebtedness to implement any or all parts of this Plan. However, any financing of redevelopment activities using tax increment revenues

shall occur prior to the 30-year time limit. Private sector redevelopment will probably occur gradually, and in the initial years, it is expected to be predominantly small in scale.

The amount of available tax increment is expected to increase as private sector redevelopment begins to take place, however for the first few years of the Plan, the low growth scenario will be most likely to occur. Investments made by the City using tax increment could include any element of this Plan, as determined by the Temple Terrace Redevelopment Agency.

Appendix

- Legal Description
- Ordinance No. 1144 creating new Future Land Use Category "Downtown Mixed Use-25", adopted May 17, 2005
- Ordinance No. 1145 changing the existing land use designation to the new "Downtown Mixed Use-25" (DMU-25) Future Land Use Category, adopted May 17, 2005
- Ordinance No. 1146 creating a Transportation Concurrency Exception Area (TCEA) in the CRA, adopted May 17, 2005
- Chapter 29 Downtown Community Redevelopment Plan Overlay Zoning District, adopted May 16, 2006

Legal Description

BEING A PORTION OF SECTIONS 22 AND 27, TOWNSHIP 28 SOUTH, RANGE 19 EAST, TEMPLE TERRACE, HILLSBOROUGH COUNTY, FLORIDA.

BEGINNING AT THE SOUTHEASTERLY CORNER OF LOT 6A OF RIVERHILLS PARK UNIT NO. 1 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 30 PAGE 57 AND THE WATERS OF THE HILLSBOROUGH RIVER; THENCE NORTHERLY ALONG THE EASTERLY LINE OF SAID LOT 6A TO THE NORTHEASTERLY CORNER THEREOF TO A POINT ON THE SOUTHERLY RIGHT-OF-WAY LINE OF RIVERHILLS DRIVE; THENCE NORTHEASTERLY ALONG SAID LINE TO A POINT OF INTERSECTION WITH THE SOUTHERLY EXTENSION OF THE WESTERLY LOT LINE OF LOT 3 OF SAID RIVERHILLS PARK; THENCE NORTHERLY ALONG SAID EASTERLY LINE TO THE SOUTHWESTERLY LOT CORNER OF LOT 12, BLOCK 2 OF GRANDVIEW ACRES UNIT N. 1 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 32 PAGE 19; THENCE NORTHERLY ALONG THE WESTERLY LINE OF SAID LOT 12 TO THE NORTHWESTERLY CORNER THEREOF, ALSO BEING THE SOUTHWESTERLY CORNER OF LOT 2 BLOCK 2; THENCE NORTHERLY ALONG THE WESTERLY LINE OF SAID LOT 2 TO THE NORTHWESTERLY CORNER THEREOF; THENCE NORTHERLY TO A POINT OF INTERSECTION OF THE NORTHERLY RIGHT-OF-WAY LINE OF BEVERLY DRIVE AND THE SOUTHERLY EXTENSION OF THE EASTERLY LINE OF LOTS 4 AND 5 BLOCK 1 OF SAID GRANDVIEW ACRES UNIT NO. 1; THENCE NORTHERLY ALONG SAID LINE TO THE SOUTHEASTERLY CORNER OF LOT 6 OF SAID BLOCK 1; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF SAID LOT 6 AND EXTENDED TO THE INTERSECTION WITH THE WESTERLY RIGHT-OF-WAY LINE OF BEVERLY DRIVE; THENCE SOUTHERLY ALONG SAID LINE TO THE SOUTHEASTERLY CORNER OF LOT 7 BLOCK 5 OF SAID GRANDVIEW ACRES UNIT NO. 1; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF LOTS 7 THROUGH 1 OF SAID BLOCK 5 AND EXTENDED TO THE SOUTHEASTERLY CORNER OF LOT 1 BLOCK 7 OF SAID GRANDVIEW ACRES UNIT NO. 2 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 32 PAGE 38; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF LOTS 1 THROUGH 5 OF SAID BLOCK 7 TO THE SOUTHWESTERLY CORNER OF LOT 5 OF SAID BLOCK 7; THENCE NORTHERLY ALONG THE WESTERLY LINE OF SAID LOT 5 TO THE NORTHWESTERLY CORNER THEREOF; THENCE NORTHERLY TO THE SOUTHWESTERLY CORNER OF LOT 1 BLOCK 2 OF TERRACE OVERLOOK NO. 1 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 12 PAGE 36; THENCE NORTHERLY ALONG THE WESTERLY LINE OF LOTS 1 THROUGH 24 AND LOT 27 OF SAID BLOCK 2 TO THE NORTHWESTERLY CORNER OF SAID 27; THENCE NORTHERLY TO THE SOUTHWESTERLY CORNER OF LOT 27 BLOCK 4 OF TERRACE OVERLOOK NO. 2 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 12 PAGE 24; THENCE NORTHERLY ALONG THE WESTERLY LINE OF LOTS 27 AND 24 THROUGH 1 OF SAID BLOCK 4 TO THE NORTHWESTERLY CORNER OF SAID LOT 1; THENCE NORTHERLY TO THE SOUTHEASTERLY CORNER OF WASHINGTON SQUARE TOWNHOME CONDOMINIUMS ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN CONDOMINIUM PLAT BOOK 6, PAGE 34; THENCE NORTHERLY ALONG THE EASTERLY LINE OF SAID CONDOMINIUM TO THE NORTHEASTERLY CORNER THEREOF SAME ALSO BEING THE SOUTHWESTERLY CORNER OF UNIVERSITY SQUARE UNIT NO. 3 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 43 PAGE 82; THENCE ALONG THE SOUTHERLY LINE OF SAID UNIVERSITY SQUARE UNIT NO. 3 TO THE NORTHEASTERLY CORNER OF A PARCEL AS DESCRIBED IN O.R. BOOK 4443 PAGE 1642; THENCE SOUTHERLY ALONG THE EASTERLY LINE OF SAID O.R. BOOK 4443 TO THE

NORTHWESTERLY CORNER OF A PARCEL AS DESCRIBED IN O.R. BOOK 8420 PAGE 1166; THENCE EASTERLY ALONG THE NORTHERLY LINE OF SAID O.R. BOOK 8420 TO THE NORTHEASTERLY CORNER THEREOF; THENCE SOUTHWESTERLY TO THE SOUTHWESTERLY CORNER OF A PARCEL AS DESCRIBED IN O.R. BOOK 3471 PAGE 185; THENCE EASTERLY ALONG THE SOUTHERLY LINE OF SAID O.R. BOOK 3471 TO THE SOUTHEASTERLY CORNER THEREOF SAID CORNER ALSO BEING THE SOUTHWESTERLY CORNER OF A PARCEL AS DESCRIBED IN O.R. BOOK 4521 PAGE 406; THENCE EASTERLY ALONG THE SOUTHERLY LINE OF SAID O.R. BOOK 4521 TO THE SOUTHEASTERLY CORNER THEREOF; THENCE EASTERLY TO THE SOUTHWESTERLY CORNER OF A PARCEL AS DESCRIBED IN O.R. BOOK 4602 PAGE 1952; THENCE EASTERLY ALONG THE SOUTHERLY LINE OF SAID O.R. BOOK 4602 TO THE SOUTHEASTERLY CORNER THEREOF, SAID CORNER ALSO BEING ON THE WESTERLY BOUNDARY OF TEMPLE HEIGHTS ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 37 PAGE 13; THENCE SOUTHERLY ALONG SAID LINE TO THE SOUTHWESTERLY CORNER OF LOT 7 OF SAID TEMPLE HEIGHTS; THENCE EASTERLY ALONG THE SOUTHERLY LINE OF SAID LOT 7 AND EXTENDED TO THE EASTERLY LINE OF SAID TEMPLE HEIGHTS; THENCE NORTHERLY ALONG SAID LINE TO THE NORTHEASTERLY CORNER THEREOF SAID CORNER ALSO BEING ON THE SOUTHERLY LINE OF TEMPLE TERRACE PLAZA ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 33 PAGE 42; THENCE WESTERLY ALONG SAID LINE TO SOUTHEASTERLY CORNER OF LOT 14 OF SAID TEMPLE TERRACE PLAZA; THENCE NORTHERLY ALONG THE EASTERLY LINE OF SAID LOT 14 TO THE NORTHEASTERLY CORNER THEREOF; THENCE EASTERLY ALONG THE NORTHERLY LINE OF LOTS 15 THROUGH 18 OF SAID TEMPLE TERRACE PLAZA AND EXTENDED TO THE EASTERLY RIGHT-OF-WAY LINE OF 56TH STREET; THENCE NORTHERLY ALONG SAID LINE TO THE SOUTHWESTERLY CORNER OF LOT 1 BLOCK 5, TEMPLE TERRACE HEIGHTS ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 31 PAGE 76; THENCE EASTERLY ALONG THE SOUTHERN LINE OF SAID BLOCK 5 TO THE SOUTHEASTERLY CORNER OF SAID BLOCK 5; THENCE SOUTHERLY ALONG THE WESTERLY RIGHT-OF-WAY LINE OF RACHEL STREET AND LOT 1 BLOCK 4 TO THE SOUTHWESTERLY CORNER OF SAID BLOCK 4; THENCE EASTERLY ALONG THE SOUTHERLY LINE OF BLOCK 4 TO THE SOUTHEASTERLY CORNER THEREOF SAID CORNER ALSO BEING ON THE WESTERLY RIGHT-OF-WAY LINE OF RIDGEDALE ROAD AND ALSO THE INTERSECTION OF THE NORTH LINE OF LOT 7 OF TEMPLE TERRACE ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 25 PAGE 68; THENCE SOUTHERLY ALONG SAID LINE TO THE INTERSECTION OF THE SOUTHERLY LINE OF LOT 8 OF SAID TEMPLE TERRACE AND SAID WESTERLY RIGHT-OF-WAY LINE; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF SAID LOT 8 TO THE NORTHEASTERLY CORNER OF TEMPLE TERRACE ESTATES ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 23 PAGE 25; THENCE SOUTHERLY ALONG THE EASTERLY LINE OF SAID TEMPLE TERRACE ESTATES TO THE P.I. OF THE SOUTHEASTERLY CORNER OF BLOCK F-10 OF SAID TEMPLE TERRACE ESTATES; THENCE SOUTHEASTERLY TO THE NORTHWESTERLY CORNER OF LOT 20 BLOCK C-3 OF TEMPLE TERRACE ESTATES ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 10 PAGE 62; THENCE SOUTHERLY ALONG THE WESTERLY LINE OF SAID LOT 20 TO THE SOUTHWESTERLY CORNER THEREOF; THENCE WESTERLY ALONG THE NORTHERLY LINE OF LOT 22 OF SAID BLOCK C-3 TO THE NORTHWESTERLY CORNER OF SAID LOT 22; THENCE SOUTHERLY ALONG THE WESTERLY LINE OF LOTS 22 THROUGH 25 OF SAID BLOCK C-3 TO THE SOUTHEASTERLY CORNER OF LOT 13 OF SAID BLOCK C-3; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF SAID LOT 13 AND EXTENDED TO THE WESTERLY RIGHT-OF-WAY LINE OF SUNNYSIDE ROAD AND THE

EASTERLY LINE OF BLOCK D-8 OF TEMPLE TERRACE ESTATES ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 17 PAGE 22; THENCE SOUTHERLY ALONG SAID LINE TO THE SOUTHEASTERLY CORNER OF SAID BLOCK D-8 AND THE NORTHERLY RIGHT-OF-WAY LINE OF SHADOWLAWN AVENUE; THENCE SOUTHWESTERLY ALONG SAID LINE AND EXTENDED TO THE EASTERLY LINE OF A PARCEL AS DESCRIBED IN O.R. 8237 BOOK 1234; THENCE SOUTHERLY ALONG SAID LINE AND EXTENDED TO THE SOUTHERLY RIGHT-OF-WAY LINE OF CHICAGO AVENUE; THENCE EASTERLY ALONG SAID LINE TO THE NORTHEASTERLY CORNER OF BLOCK D-14 OF TEMPLE TERRACE ESTATES ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 17 PAGE 22; SAME ALSO BEING THE WESTERLY RIGHT-OF-WAY LINE OF SPRING DELE PLACE; THENCE SOUTHERLY ALONG SAID LINE AND EXTENDED TO THE NORTHEASTERLY LINE OF TEMPLE TERRACE ESTATES ACCORDING TO MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 10 PAGE 67; THENCE SOUTHEASTERLY ALONG THE EASTERLY LINE OF SAID SUBDIVISION TO THE EASTERLY CORNER OF LOT 10 OF SAID SUBDIVISION; THENCE SOUTHWESTERLY ALONG THE SOUTHEASTERLY LINE OF LOTS 10 AND 7 OF SAID SUBDIVISION TO THE SOUTHERLY CORNER THEREOF; THENCE SOUTHEASTERLY ALONG THE SOUTHWESTERLY LINE OF LOT 8 OF SAID SUBDIVISION TO A POINT OF INTERSECTION WITH THE NORTHEASTERLY EXTENSION OF THE NORTHERLY LINE OF LOT 1, RIVER TERRACE ESTATES ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 53 PAGE 15; THENCE SOUTHWESTERLY ALONG SAID LINE TO THE NORTHWESTERLY CORNER THEREOF AND THE WATERS OF THE HILLSBOROUGH RIVER; THENCE MEANDERING NORTHWESTERLY ALONG THE WATERS OF THE HILLSBOROUGH RIVER TO THE AFOREDESCRIBED POINT OF BEGINNING AND BINDING THEREWITH TO.

ORDINANCE NO. 1144

AN ORDINANCE OF THE CITY OF TEMPLE TERRACE, FLORIDA, AMENDING THE CITY'S COMPREHENSIVE PLAN FUTURE LAND USE ELEMENT AND MAP, BY CREATING A NEW FUTURE LAND USE DESIGNATION, KNOWN AS "DOWNTOWN MIXED USE-25" (DMU-25), AND RELATED POLICIES, APPLICABLE TO AND CHANGING THE EXISTING LAND USE DESIGNATIONS WITHIN THE 225.31+/- ACRES IN THE CITY'S COMMUNITY REDEVELOPMENT AREA (CRA) TO ALLOW CONSIDERATION FOR NEW DEVELOPMENT OF AN URBAN CHARACTER, PURSUANT TO THE CITY'S REDEVELOPMENT MASTER PLAN; BY PROVIDING A DESCRIPTION AND DEFINITION OF THE DOWNTOWN MIXED USE-25 PLAN CATEGORY; BY AMENDING AND ENACTING OBJECTIVES AND POLICIES RELATIVE TO THE DOWNTOWN MIXED USE-25 PLAN CATEGORY; PROVIDING A SEPARABILITY CLAUSE, EFFECTIVE DATE, AND REPEALING ALL ORDINANCES OR PARTS OF ORDINANCES IN CONFLICT HEREWITH.

WHEREAS, the City of Temple Terrace Comprehensive Plan was adopted by Ordinance No. 694 on July 18, 1989; and

WHEREAS, the City Council has determined that an amendment to the City's Comprehensive Plan Future Land Use Element and Map is required to create a new future land use designation, known as "Downtown Mixed Use-25" (DMU-25), and related policies, applicable to and changing the existing land use designations within the 225.31+/- acres in the City's Community Redevelopment Area (CRA) to allow consideration for new development of an urban character, pursuant to the city's redevelopment master plan; and

WHEREAS, this amendment is consistent with the Future Land Use Element of the adopted Comprehensive Plan,

NOW, THEREFORE, BE IT ORDAINED BY THE MAYOR AND CITY COUNCIL OF THE CITY OF TEMPLE TERRACE, FLORIDA:

Section 1. The City's Comprehensive Future Land Use Element, adopted by Ordinance No. 694, is hereby amended by creating a new future land use Plan Category, known as "Downtown Mixed Use-25" (DMU-25), allowing mixed use developments of an urban character in relation to the established downtown Community Redevelopment Area (CRA).

Section 2. The City's Comprehensive Future Land Use Element, adopted by Ordinance No. 694, is hereby amended by enactment of the following description for the use of Downtown Mixed Use-25:

DOWNTOWN MIXED USE-25

Description:

Purpose: The Downtown Mixed Use-25 (DMU-25) land use category applies to the City's established downtown Community Redevelopment Area (CRA) and allows for a complementary mix and range of land uses. The purpose of the Downtown Mixed Use-25 category is to establish, define, promote, and facilitate the redevelopment and enhancement of the City's primary and historical urban center and its traditional downtown central business district

surrounding the Busch Boulevard/Bullard Parkway and N. 56th Street intersection. The DMU-25 is intended to foster development incentives through increased densities and intensities but subject to adherence to design guidelines and requirements in order that new development and redevelopment will prevent, replace, and eliminate blight, and the deterioration of physical conditions associated with outmoded, inefficient, non-viable public and private design and development which is unable to serve the long term community interests. The DMU-25 land use category is further intended to implement the downtown and general redevelopment goals, objectives and policies of the Future Land Use Element in that area, as well as to promote the accomplishment of the City's Community Redevelopment Area (CRA) designation for the downtown area and the implementation of redevelopment plans, studies and regulations.

Density/Intensity: Residential uses may range from single family detached to multi-family, and for purposes of density will allow consideration of a maximum average density of 25 dwelling units per gross acre within the DMU-25 designated areas within the overall CRA boundary. Densities on individual parcels may exceed this amount as long as the gross density within the overall DMU-25 designated area within the CRA boundary does not exceed 25 dwelling units per gross acre. The preferred residential use is medium to high density multi-family residential or mixed use non-residential with multi-family residential, subject to compatibility with and appropriate transition considerations to adjacent low density residential uses.

Up to a maximum average of 1.5 floor area ratio (FAR) may be considered within the overall DMU-25 designated area within the CRA area, but a 3.5 FAR maximum average within the DMU-25 land use category may be permitted for individual projects. When above-grade parking structures are required or considered for an individual project, a maximum average 4.5 FAR shall be applicable, as long as intensities within the overall DMU-25 designated area within the CRA boundary shall not exceed a maximum average of 1.5 FAR.

Notwithstanding the above statements regarding density and intensity limits, an additional limit applicable to the downtown CRA, as designated by the DMU-25 land use category, shall be an absolute limit of 3,500,000 square feet of non-residential floor area, and an absolute limit of 3,500 dwelling units. As applied to the 225+/- acre downtown CRA, where existing residential dwelling units and non-residential square footage occurs, this limit should generate a net increase of less than 2,400,000 square feet non-residential floor area and 2,400 dwelling units. This net increase assumes that every potential development option for the CRA is exercised, including conversion of school sites and other public/semi-public and park lands into mixed use development over time. Because it is unlikely that such conversions will occur, and given the development configuration of the CRA, it is realistically anticipated that the actual net development impact which will occur for the entire CRA will be 1,200,000 square feet of floor area of non-residential uses and 2,400 dwelling units. These more realistic net development expectations offer the better figures for which impacts on levels of service for various public facilities should be assessed, at least for the next 5-10 years (2005-2010).

Permitted Uses: The Downtown Mixed Use-25 (DMU-25) allows and encourages a mixture and range of uses generally associated with or considered with traditional urban core central business districts, particularly those that are pedestrian friendly, and not necessarily those catering to automobile traffic. Proposed uses within the area encourage the development of a lively, mixed-use, walkable downtown that effectively serves Temple Terrace residents, merchants, visitors, and employees. Uses include: commercial, retail, office and office services, personal and professional services, restaurants, parks, recreation and entertainment venues, institutional, postal facilities, public and semi-public, medium and high density residential and mixed-use structures that combine these uses. Heavy commercial uses associated with automobiles, such as gasoline stations, auto repair facilities, and motorized vehicle rentals or sales may be considered but are generally not encouraged. Industrial, warehouse, transportation servicing, storage (inside and outside), manufacturing, general wholesaling, agriculture uses and uses involving high levels or significant concentrations of hazardous materials are not allowed; the notable exceptions are generally small-scale dry cleaners and photographic developing. These uses are listed for clarification purposes in providing for new development through rezonings pursuant to the DMU-25 category. Existing uses and zonings in place, when the DMU-25 land use category is first applied to the CRA, are considered acceptable and supported by the DMU-25

designation. Improvements to and maintenance of such uses are considered appropriate and acceptable under DMU-25. The strict application of new zoning and new implementing design and development standards created for the CRA under DMU-25 are intended to ensure that new development shall occur pursuant to and in strict compliance with the adopted master plan(s) commissioned, prepared, or submitted and adopted by the City, to achieve the redevelopment of the downtown envisioned by the City over time.

Land use types shall be permitted according to the following schedule, expressed as a percentage of the total area in this plan category. The percentages shall be applied on an area-wide basis within the 225+/- acre DMU-25 area, but shall not be interpreted to require development with a mix of uses for any single project, although such mixture is encouraged. The percentages are one of the means by which the density and intensity maximums and build-out assumptions were calculated, and those limitations provide the primary determinant of the extent of non-residential square footage and of the number of residential dwelling units ultimately allowed within the 225+/- acre DMU-25 area of the CRA.

Development under all of these limitations shall be monitored by the City of Temple Terrace and the Hillsborough County City-County Planning Commission to ensure that the limitations are not exceeded.

In no case shall any one development or accumulation of developments be approved by the City such that the development limits are exhausted, as long as there remain private developable parcels within the CRA, which over time may be redeveloped to achieve the master plan envisioned for the entire 225+/- acre CRA.

Maximum Allowable Percentages of Land Use Acreage or Total Dedicated Square Footage
Within the 225+/- Acre DMU-25 Area:

Residential: 75%

Commercial (including office development, public/semi-public, and other non-residential and non-industrial Uses): *25%*

Industrial: 0%

Permitted Zoning: The Downtown Mixed Use-25 allows consideration of a mixture and range of zoning classifications but requires new development or substantial expansion of existing development to be site plan controlled PD-Planned Development, or similar zoning expressly developed to implement the downtown redevelopment plan, and subject to any special or overlay zoning districts developed for or applying to the CRA or its parts.

Uses associated with existing zoning, including CG (Commercial General), CO (Commercial Office), R-10 (Single Family Residential), R-7.5 (Single Family Residential), and R-MF (Multi-Family Residential), are expressly allowed as presently applied; boundary adjustments and reconstruction, rehabilitation, or new construction within these existing zoned areas or expansion of these zoning classifications to adjacent parcels, which clearly and demonstrably furthers the intent and purposes of the DMU-25 land use category and the City's downtown redevelopment plan shall be allowed. However, the application of any allowed zoning classifications and site plans and development thereunder shall be compatible with and in furtherance of the downtown redevelopment plan, particularly in terms of requiring and ensuring compatibility with and transition to adjacent users, and especially with low density residential neighborhoods adjacent to the DMU-25 area.

Relationship to Other Areas of Use and the Street System:

- Downtown Mixed Use-25 areas should be confined to locations within the City's Community Redevelopment Area (CRA) as shown on the Future Land Use Map.

- Downtown Mixed Use-25 area shall establish, define, develop and enhance the City's primary and historical urban center core and its traditional downtown central business district with special emphasis on uniformity of signage, site lighting standards and fixtures, façade treatment, color palette, architectural detail, building placement and orientation, landscaping, pedestrian amenities, and similar detailed design standards by diminishing and transitioning residential densities and non-residential intensities at the edges of the area in a manner that complements and is compatible with adjacent land uses and which creates opportunities for adjacent uses to easily access and relate to the area.
- Commercial uses, including office development and major public and semi-public uses shall be located and designed to take advantage of the proximity of arterial and collector roadways and intersections for vehicular access in a manner that directs or circulates heavy vehicular traffic within or peripheral to the main downtown area and away from local roads and adjacent residential neighborhoods, especially those outside the DMU-25 designated area.
- Strip development with separate individual driveway access for commercial uses to arterials should be discouraged for new development.
- Perimeters shall feature transitional development which may include compatible land uses, special street frontage treatments, open space, landscaping, and/or visual screening.
- Opportunities for pedestrian, bicycle, or similar low impact access and circulation should be provided as part of project design. Design techniques should be included that enhance and support pedestrian and bicycle facilities, and provide for connections to adjacent commercial areas and surrounding residential neighborhoods.

Relationship to Facilities and Services:

- Water and sewer service should be available or programmed for the area.
- Fire, emergency medical services (EMS), and police protection shall be available and adequate.
- Public spaces should be provided for leisure opportunities and integrated into overall design. Open space should be designed to provide or enhance the urban experience with unique recreational or pedestrian-oriented amenities.

Relationship to Natural Resources and Features:

- Well-drained areas where soils are suitable for structures, streets and parking.
- Development and redevelopment should take advantage of existing land contour, tree cover, water features, and views, or should create them, to enhance the urban experience and promote diversity within the site design, especially where they create a scenic amenity, or enhance the transition to and compatibility with adjacent low density residential neighborhoods. Site plan controls should ensure the maximum retention of these features within the constraints of project design.
- Landscaping and streetscape features should be provided and integrated into an overall design, including street tree plantings and landscape medians.

Section 3. The City's Comprehensive Future Land Use Element, adopted by Ordinance No. 694, is hereby amended by enactment of the following *Definition* for the "Downtown Mixed Use-25" (DMU-25) Plan Category in the Definitions Section:

Downtown Mixed Use-25 (DMU-25) – The land use category used to establish, define, promote, and facilitate the redevelopment and enhancement of the City's primary and historical urban center and its traditional downtown central business district surrounding the Busch Boulevard/Bullard Parkway and N. 56th Street intersection. The DMU-25 is intended to foster development incentives through increased densities and intensities but subject to adherence to design guidelines and requirements in order that new development and redevelopment will prevent, replace, and eliminate blight, and the deterioration of physical conditions associated with outmoded, inefficient, non-

viable public and private design and development which is unable to serve the long term community interests. The DMU-25 land use category is further intended to implement the downtown and general redevelopment goals, objectives and policies of the Future Land Use Element in that area, as well as to promote the accomplishment of the City's Community Redevelopment Area (CRA) designation for the downtown area and the implementation of redevelopment plans, studies and regulations.

Section 4. The City's Comprehensive Future Land Use Element, adopted by Ordinance No. 694, is hereby amended by repealing Policy 1.3.2 for Recreational Development in the Neighborhood Conservation section of the Goals, Objectives and Policies and by enacting in its place a new Policy 1.3.2:

Policy 1.3.2:

Acquire vacant waterfront parcels as funding is approved through local, state, and federal programs, where feasible; and retain for public purposes, including downtown CRA redevelopment related purposes when pursuant to a master plan, as implemented through the DMU-25 land use category.

Section 5. The City's Comprehensive Future Land Use Element, adopted by Ordinance No. 694, is hereby amended by repealing Policy 2.1.3 for Non-Residential Development in the Community Development section of the Goals, Objectives and Policies and by enacting in its place a new Policy 2.1.3:

Policy 2.1.3:

Buffer existing residential uses from the negative impacts of non-residential development (physical, visual, or auditory), by assuring that these negative impacts are accommodated by appropriate design. Recognize that residential development within the downtown CRA, especially DMU-25 designated areas, may be more urban in character, where differing uses are mixed or only separated by service alleys or similar conscious designs.

Section 6. The City's Comprehensive Future Land Use Element, adopted by Ordinance No. 694, is hereby amended by repealing Objective 2.3 and Policy 2.3.1 for Non-Residential Development in the Community Development section of the Goals, Objectives and Policies and by enacting in their place, a new Objective 2.3 and a new Policy 2.3.1:

Objective 2.3: Conduct traffic studies when necessary to determine the impact of non-residential traffic on the City's neighborhoods.

Policy 2.3.1:

Protect residential neighborhoods from cut-through traffic. Recognize that the downtown CRA, especially the DMU-25 designated areas, is planned to be more urban in character, and promote a more integrated network of streets throughout the CRA, and its immediate environs, to provide multiple routes to any destination, reducing the need for any one street to carry all of the traffic.

Section 7: The City's Comprehensive Future Land Use Element, adopted by Ordinance No. 694, is hereby amended by repealing Objective 3.2 and Policies 3.2.1 and 3.2.2 in the Downtown Redevelopment section of the Goals, Objectives and Policies and by enacting in their place, a new Objective 3.2 and new Policies 3.2.1 and 3.2.2 for Downtown Redevelopment:

Objective 3.2: The City shall amend the Land Development Code to adopt Urban Design Guidelines for all new development and redevelopment within the designated downtown CRA by December 31, 2005, to implement the DMU-25 land use category and the master plan for the CRA and refinements thereto and to portions thereof. Incentive programs and urban design guidelines will be used to ensure private and public participation in the development of the downtown area.

Policy 3.2.1:

Develop and implement landscaping and streetscape plans adjacent to the arterial road rights-of-way that include street tree plantings, landscaped medians, pavement treatments, lighting, street furniture, etc., in the redevelopment area, especially the DMU-25 designated areas of the CRA.

Policy 3.2.2:

Encourage high-quality site designs through the City's Land Development Code and Urban Design Guidelines that provide incentives and specific development standards for on-site lighting, perimeter landscaping, and signage, and require it for DMU-25 designated areas within the City's downtown CRA. As much as feasible, parking lots shall be relegated to the interior blocks (screened from streets by buildings or landscape). Access and egress shall be designed to conform and connect with the surrounding street network.

Section 8. The City's Comprehensive Future Land Use Element, adopted by Ordinance No. 694, is hereby amended by enacting Objective 3.4 and Policies 3.4.1, 3.4.2, 3.4.3, 3.4.4, 3.4.5, 3.4.6, and 3.4.7 in the Downtown Redevelopment section of the Goals, Objectives and Policies:

Objective 3.4: The Downtown Mixed Use-25 designation for the 225+/- acre downtown Community Redevelopment Area (CRA) of the City shall consider intergovernmental and regional issues, including housing, environment, economic development, transportation, and emergency preparedness and hazard mitigation.

Policy 3.4.1:

DMU-25 areas shall consider and coordinate with adjacent local government's plans, and shall specifically address impacts on land uses along its boundary with the City of Tampa and ensure that site-planned developments approved pursuant to DMU-25 are designed to be compatible with existing and future planned uses and appropriately transition and mitigate impacts directly adjacent to uses within the City of Tampa.

Policy 3.4.2:

DMU-25 areas shall be developed with a special sensitivity to and regulations for its location bordering on that portion of the Hillsborough River used by the City of Tampa as a water reservoir supplying its potable water treatment facilities further downriver at the Tampa waterworks dam. Continued use of such mitigation strategies for stormwater drainage as the innovative Chicago Avenue drainage containment and treatment facility regulating the quality of stormwater outfall into the River shall be encouraged.

Policy 3.4.3:

The City shall consider affordable housing strategies, and the special opportunity occasioned by mixed use land use categories, when the DMU-25 area is developed, to ensure that affordable housing, such as presently exists through such affordable projects as the 308 units of The Oaks and the Brentwood apartment complexes, and the 252 units of the mid-level Doral Oaks apartments, continues to be a viable part of the downtown CRA.

Policy 3.4.4:

Through the application of the DMU-25 land use to the development of the City's downtown CRA, the City shall strive to make the economic development impact of this redevelopment an economic enhancement not only of the City itself but of the larger region by considering its benefit as another type of housing alternative for military personnel at MacDill AFB, professors and employees of the University of South Florida and its medical and research facilities, and an attractive urban lifestyle amenity for those associated with the arts and similar creative enterprises.

Policy 3.4.5:

The City shall continue to use the opportunity afforded by redevelopment through the DMU-25 land use in the CRA to support the Busch Boulevard beautification project, recognizing the added amenity it provides as both a gateway and destination project complementary to Busch Boulevard and its role as a significant tourist corridor and to Busch Gardens as a major tourist facility of the region.

Policy 3.4.6:

The City shall continue to promote and utilize the design features associated with the DMU-25 category, as applied to the CRA, in furthering the multi-modal transportation aspects inherent in the master plan for the CRA.

Policy 3.4.7:

Because the DMU-25 area and the CRA do not lie in a Coastal High Hazard Area, and because it is located on that portion of the Hillsborough River most protected from flooding by the City of Tampa's dam and the flood control features of the two canals leading from the River to the Tampa Bypass Canal, the City shall strive to recognize and utilize the CRA's geographic advantages; and it shall consider opportunities for aiding the regional hurricane and emergency preparedness and hazard mitigation efforts through design enhancements which might offer evacuation and shelter options in emergency or disaster conditions.

Section 9. The City's Comprehensive Future Land Use Element, adopted by Ordinance No. 694, is hereby amended by repealing Policy 6.1.3 for General Development in the Growth Management section of the Goals, Objectives and Policies and enacting in its place a new Policy 6.1.3:

Policy 6.1.3:

Concentrate and infill new development on vacant land with priority given to redevelopment areas where facility capacity is available, especially within the DMU-25's designated areas of the CRA.

Section 10. The City's Comprehensive Future Land Use Element, adopted by Ordinance No. 694, is hereby amended by enacting Policy 6.2.9 for Redevelopment Strategies in the Growth Management section of the Goals, Objectives and Policies:

Policy 6.2.9:

envisioned environment shall be reflective of the downtown studies and plans commissioned by the City through private consultants and prepared with the substantial participation and input of the City's citizens during 2004, as subsequently adopted.

Section 11. The City's Comprehensive Future Land Use Element, adopted by Ordinance No. 694, is hereby amended by enacting Policy 6.8.6 in the Intergovernmental Coordination section of the Goals, Objectives and Policies:

Policy 6.8.6:

The City of Temple Terrace will partner with and assist the Hillsborough County School Board in providing adequate capacity within local public schools impacted by a net increase in dwelling units approved by the City within the CRA, as a result of redevelopment pursuant to DMU-25 and related development orders. This may include such strategies as establishing provisions in the Land Development Code for dedicating private land for new classrooms, setting aside public land for new classrooms, and other creative and innovative techniques which the City and the School Board may agree upon as mitigating the impact of CRA redevelopment on local schools.

Section 12. If any part of this ordinance is declared invalid by a court of competent jurisdiction, such part or parts shall be severable and the remaining part or parts hereof shall continue to be in full force and effect.

Section 13. This ordinance shall take effect immediately upon its passage, approval, and being posted or published as required by law.

Section 14. All ordinances or parts of ordinances not specifically in conflict herewith are hereby continued in full force and effect, but all ordinances or parts of ordinances in conflict herewith are hereby repealed.

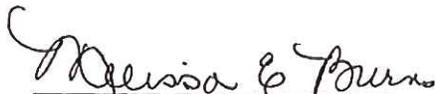
PASSED AND ORDAINED BY THE CITY COUNCIL OF THE CITY OF TEMPLE TERRACE, FLORIDA, THIS 17th DAY OF May, 2005.

APPROVED BY THE MAYOR THIS 17th DAY OF May, 2005.



ATTEST:


JOSEPH A. AFFRONTI, SR., MAYOR


MELISSA E. BURNS, CMC
CITY CLERK

ORDINANCE NO. 1145

AN ORDINANCE OF THE CITY OF TEMPLE TERRACE, FLORIDA, AMENDING THE CITY'S COMPREHENSIVE LAND USE PLAN MAP AS IT RELATES TO PROPERTY LOCATED WITHIN THE BOUNDARIES OF THE APPROXIMATE 225.31 ACRES OF THE CITY'S COMMUNITY REDEVELOPMENT AREA (CRA), SURROUNDING THE INTERSECTION OF 56TH STREET AND BUSCH BOULEVARD/BULLARD PARKWAY, LEGALLY DESCRIBED IN EXHIBIT "A" ATTACHED HERETO AND MADE A PART HEREOF, BY CHANGING THE EXISTING LAND USE DESIGNATIONS ON SAID PROPERTY TO THE NEW DOWNTOWN MIXED USE-25 (DMU-25) FUTURE LAND USE CATEGORY, CHANGING THE EXISTING LAND USE DESIGNATIONS, AND ADDING TO THE FUTURE LAND USE MAP THE BOUNDARIES OF THE CRA, THE BOUNDARIES OF THE PROPOSED TRANSPORTATION CONCURRENCY EXCEPTION AREA (TCEA), WHICH ARE THE SAME AS THE CRA BOUNDARIES, AND THE NEW DMU-25 LAND USE CATEGORY; PROVIDING A SEPARABILITY CLAUSE, EFFECTIVE DATE, AND REPEALING ALL ORDINANCES OR PARTS OF ORDINANCES IN CONFLICT HEREWITH.

WHEREAS, the City of Temple Terrace Comprehensive Plan was adopted by Ordinance No. 694 on July 18, 1989; and

WHEREAS, the City Council has determined that an amendment to the City's Comprehensive Land Use Plan Map is required to change the City's Future Land Use Map by designating the area within the boundaries of the approximate 225.31 acres of the City's Community Redevelopment Area (CRA), surrounding the intersection of 56th Street and Busch Boulevard/Bullard Parkway, to the new Downtown Mixed Use-25 (DMU-25) land use category, changing the existing land use designations, and adding to the Future Land Use Map the boundaries of the CRA, the boundaries of the proposed Transportation Concurrency Exception Area (TCEA), which are the same as the CRA boundaries, and the new DMU-25 land use category; and

WHEREAS, the property located within the boundaries of the approximate 225.31 acres of the City's Community Redevelopment Area (CRA), surrounding the intersection of 56th Street and Busch Boulevard/Bullard Parkway is legally described in Exhibit "A" attached hereto and made a part hereof,

NOW, THEREFORE, BE IT ORDAINED BY THE MAYOR AND CITY COUNCIL OF THE CITY OF TEMPLE TERRACE, FLORIDA:

Section 1. The City's Comprehensive Land Use Plan Map, adopted by Ordinance No. 694, is hereby amended by changing the City's Future Land Use Map by designating the area within the boundaries of the approximate 225.31 acres of the City's Community Redevelopment Area (CRA), surrounding the intersection of 56th Street and Busch Boulevard/Bullard Parkway, to the new Downtown Mixed Use-25 (DMU-25) land use category, changing the existing land use designations, and adding to the Future Land Use Map the boundaries of the CRA, the boundaries of the proposed Transportation Concurrency Exception Area (TCEA), which are the same as the CRA boundaries, and the new DMU-25 land use category, legally described in Exhibit "A," attached hereto and made a part hereof.

Section 2. If any part of this ordinance is declared invalid by a court of competent jurisdiction, such part or parts shall be severable and the remaining part or parts hereof shall continue to be in full force and effect.

Section 3. This ordinance shall take effect immediately upon its passage, approval, and being posted or published as required by law.

Section 4. All ordinances or parts of ordinances not specifically in conflict herewith are hereby continued in full force and effect, but all ordinances or parts of ordinances in conflict herewith are hereby repealed.

PASSED AND ORDAINED BY THE CITY COUNCIL OF THE CITY OF TEMPLE TERRACE,
FLORIDA, THIS 17th DAY OF May, 2005.

APPROVED BY THE MAYOR THIS 17th DAY OF May, 2005.



Joseph A. Affronti, Sr.
JOSEPH A. AFFRONTI, SR., MAYOR

ATTEST:

Melissa E. Burns
MELISSA E. BURNS, CMC
CITY CLERK

EXHIBIT "A"

CPA-04-09 – (DMU-25) – Map Amendment

LEGAL DESCRIPTION:

BEING A PORTION OF SECTIONS 22 AND 27, TOWNSHIP 28 SOUTH, RANGE 19 EAST, TEMPLE TERRACE, HILLSBOROUGH COUNTY, FLORIDA.

BEGINNING AT THE SOUTHEASTERLY CORNER OF LOT 6A OF RIVERHILLS PARK UNIT NO. 1 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 30 PAGE 57 AND THE WATERS OF THE HILLSBOROUGH RIVER; THENCE NORTHERLY ALONG THE EASTERLY LINE OF SAID LOT 6A TO THE NORTHEASTERLY CORNER THEREOF TO A POINT ON THE SOUTHERLY RIGHT-OF-WAY LINE OF RIVERHILLS DRIVE; THENCE NORTHEASTERLY ALONG SAID LINE TO A POINT OF INTERSECTION WITH THE SOUTHERLY EXTENSION OF THE WESTERLY LOT LINE OF LOT 3 OF SAID RIVERHILLS PARK; THENCE NORTHERLY ALONG SAID EASTERLY LINE TO THE SOUTHWESTERLY LOT CORNER OF LOT 12, BLOCK 2 OF GRANDVIEW ACRES UNIT N. 1 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 32 PAGE 19; THENCE NORTHERLY ALONG THE WESTERLY LINE OF SAID LOT 12 TO THE NORTHWESTERLY CORNER THEREOF, ALSO BEING THE SOUTHWESTERLY CORNER OF LOT 2 BLOCK 2; THENCE NORTHERLY ALONG THE WESTERLY LINE OF SAID LOT 2 TO THE NORTHWESTERLY CORNER THEREOF; THENCE NORTHERLY TO A POINT OF INTERSECTION OF THE NORTHERLY RIGHT-OF-WAY LINE OF BEVERLY DRIVE AND THE SOUTHERLY EXTENSION OF THE EASTERLY LINE OF LOTS 4 AND 5 BLOCK 1 OF SAID GRANDVIEW ACRES UNIT NO. 1; THENCE NORTHERLY ALONG SAID LINE TO THE SOUTHEASTERLY CORNER OF LOT 6 OF SAID BLOCK 1; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF SAID LOT 6 AND EXTENDED TO THE INTERSECTION WITH THE WESTERLY RIGHT-OF-WAY LINE OF BEVERLY DRIVE; THENCE SOUTHERLY ALONG SAID LINE TO THE SOUTHEASTERLY CORNER OF LOT 7 BLOCK 5 OF SAID GRANDVIEW ACRES UNIT NO. 1; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF LOTS 7 THROUGH 1 OF SAID BLOCK 5 AND EXTENDED TO THE SOUTHEASTERLY CORNER OF LOT 1 BLOCK 7 OF SAID GRANDVIEW ACRES UNIT NO. 2 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 32 PAGE 38; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF LOTS 1 THROUGH 5 OF SAID BLOCK 7 TO THE SOUTHWESTERLY CORNER OF LOT 5 OF SAID BLOCK 7; THENCE NORTHERLY ALONG THE WESTERLY LINE OF SAID LOT 5 TO THE NORTHWESTERLY CORNER THEREOF; THENCE NORTHERLY TO THE SOUTHWESTERLY CORNER OF LOT 1 BLOCK 2 OF TERRACE OVERLOOK NO. 1 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 12 PAGE 36; THENCE NORTHERLY ALONG THE WESTERLY LINE OF LOTS 1 THROUGH 24 AND LOT 27 OF SAID BLOCK 2 TO THE NORTHWESTERLY CORNER OF SAID 27; THENCE NORTHERLY TO THE SOUTHWESTERLY CORNER OF LOT 27 BLOCK 4 OF TERRACE OVERLOOK NO. 2 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 12 PAGE 24; THENCE NORTHERLY ALONG THE WESTERLY LINE OF LOTS 27 AND 24 THROUGH 1 OF SAID BLOCK 4 TO THE NORTHWESTERLY CORNER OF SAID LOT 1; THENCE NORTHERLY TO THE SOUTHEASTERLY CORNER OF WASHINGTON SQUARE TOWNHOME CONDOMINIUMS ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN CONDOMINIUM PLAT BOOK 6, PAGE 34; THENCE NORTHERLY ALONG THE EASTERLY LINE OF SAID CONDOMINIUM TO THE NORTHEASTERLY CORNER THEREOF SAME ALSO BEING THE SOUTHWESTERLY CORNER OF UNIVERSITY SQUARE UNIT NO. 3 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 43 PAGE 82; THENCE ALONG THE SOUTHERLY LINE OF SAID UNIVERSITY SQUARE UNIT NO. 3 TO THE NORTHEASTERLY CORNER OF A PARCEL AS DESCRIBED IN O.R. BOOK 4443 PAGE 1642; THENCE SOUTHERLY ALONG THE EASTERLY LINE OF SAID O.R. BOOK 4443 TO THE NORTHWESTERLY CORNER OF A PARCEL AS DESCRIBED IN O.R. BOOK 8420 PAGE 1166; THENCE EASTERLY ALONG THE NORTHERLY LINE OF SAID

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O.R. BOOK 8420 TO THE NORTHEASTERLY CORNER THEREOF; THENCE SOUTHWESTERLY TO THE SOUTHWESTERLY CORNER OF A PARCEL AS DESCRIBED IN O.R. BOOK 3471 PAGE 185; THENCE EASTERLY ALONG THE SOUTHERLY LINE OF SAID O.R. BOOK 3471 TO THE SOUTHEASTERLY CORNER THEREOF SAID CORNER ALSO BEING THE SOUTHWESTERLY CORNER OF A PARCEL AS DESCRIBED IN O.R. BOOK 4521 PAGE 406; THENCE EASTERLY ALONG THE SOUTHERLY LINE OF SAID O.R. BOOK 4521 TO THE SOUTHEASTERLY CORNER THEREOF; THENCE EASTERLY TO THE SOUTHWESTERLY CORNER OF A PARCEL AS DESCRIBED IN O.R. BOOK 4602 PAGE 1952; THENCE EASTERLY ALONG THE SOUTHERLY LINE OF SAID O.R. BOOK 4602 TO THE SOUTHEASTERLY CORNER THEREOF, SAID CORNER ALSO BEING ON THE WESTERLY BOUNDARY OF TEMPLE HEIGHTS ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 37 PAGE 13; THENCE SOUTHERLY ALONG SAID LINE TO THE SOUTHWESTERLY CORNER OF LOT 7 OF SAID TEMPLE HEIGHTS; THENCE EASTERLY ALONG THE SOUTHERLY LINE OF SAID LOT 7 AND EXTENDED TO THE EASTERLY LINE OF SAID TEMPLE HEIGHTS; THENCE NORTHERLY ALONG SAID LINE TO THE NORTHEASTERLY CORNER THEREOF SAID CORNER ALSO BEING ON THE SOUTHERLY LINE OF TEMPLE TERRACE PLAZA ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 33 PAGE 42; THENCE WESTERLY ALONG SAID LINE TO SOUTHEASTERLY CORNER OF LOT 14 OF SAID TEMPLE TERRACE PLAZA; THENCE NORTHERLY ALONG THE EASTERLY LINE OF SAID LOT 14 TO THE NORTHEASTERLY CORNER THEREOF; THENCE EASTERLY ALONG THE NORTHERLY LINE OF LOTS 15 THROUGH 18 OF SAID TEMPLE TERRACE PLAZA AND EXTENDED TO THE EASTERLY RIGHT-OF-WAY LINE OF 56TH STREET; THENCE NORTHERLY ALONG SAID LINE TO THE SOUTHWESTERLY CORNER OF LOT 1 BLOCK 5, TEMPLE TERRACE HEIGHTS ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 31 PAGE 76; THENCE EASTERLY ALONG THE SOUTHERN LINE OF SAID BLOCK 5 TO THE SOUTHEASTERLY CORNER OF SAID BLOCK 5; THENCE SOUTHERLY ALONG THE WESTERLY RIGHT-OF-WAY LINE OF RACHEL STREET AND LOT 1 BLOCK 4 TO THE SOUTHWESTERLY CORNER OF SAID BLOCK 4; THENCE EASTERLY ALONG THE SOUTHERLY LINE OF BLOCK 4 TO THE SOUTHEASTERLY CORNER THEREOF SAID CORNER ALSO BEING ON THE WESTERLY RIGHT-OF-WAY LINE OF RIDGEDALE ROAD AND ALSO THE INTERSECTION OF THE NORTH LINE OF LOT 7 OF TEMPLE TERRACE ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 25 PAGE 68; THENCE SOUTHERLY ALONG SAID LINE TO THE INTERSECTION OF THE SOUTHERLY LINE OF LOT 8 OF SAID TEMPLE TERRACE AND SAID WESTERLY RIGHT-OF-WAY LINE; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF SAID LOT 8 TO THE NORTHEASTERLY CORNER OF TEMPLE TERRACE ESTATES ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 23 PAGE 25; THENCE SOUTHERLY ALONG THE EASTERLY LINE OF SAID TEMPLE TERRACE ESTATES TO THE P.I. OF THE SOUTHEASTERLY CORNER OF BLOCK F-10 OF SAID TEMPLE TERRACE ESTATES; THENCE SOUTHEASTERLY TO THE NORTHWESTERLY CORNER OF LOT 20 BLOCK C-3 OF TEMPLE TERRACE ESTATES ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 10 PAGE 62; THENCE SOUTHERLY ALONG THE WESTERLY LINE OF SAID LOT 20 TO THE SOUTHWESTERLY CORNER THEREOF; THENCE WESTERLY ALONG THE NORTHERLY LINE OF LOT 22 OF SAID BLOCK C-3 TO THE NORTHWESTERLY CORNER OF SAID LOT 22; THENCE SOUTHERLY ALONG THE WESTERLY LINE OF LOTS 22 THROUGH 25 OF SAID BLOCK C-3 TO THE SOUTHEASTERLY CORNER OF LOT 13 OF SAID BLOCK C-3; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF SAID LOT 13 AND EXTENDED TO THE WESTERLY RIGHT-OF-WAY LINE OF SUNNYSIDE ROAD AND THE EASTERLY LINE OF BLOCK D-8 OF TEMPLE TERRACE ESTATES ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 17 PAGE 22; THENCE SOUTHERLY ALONG SAID LINE TO THE SOUTHEASTERLY CORNER OF SAID BLOCK D-8 AND THE NORTHERLY RIGHT-OF-WAY LINE OF SHADOWLAWN AVENUE; THENCE SOUTHWESTERLY ALONG SAID LINE AND EXTENDED TO THE EASTERLY LINE OF A PARCEL AS DESCRIBED IN O.R. 8237 BOOK 1234; THENCE SOUTHERLY ALONG SAID LINE AND EXTENDED TO THE SOUTHERLY RIGHT-OF-WAY LINE OF CHICAGO AVENUE; THENCE EASTERLY ALONG SAID LINE TO THE NORTHEASTERLY CORNER OF BLOCK D-14 OF TEMPLE TERRACE ESTATES ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 17 PAGE 22; SAME ALSO BEING THE WESTERLY RIGHT-OF-WAY LINE OF SPRING DELE PLACE; THENCE

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SOUTHERLY ALONG SAID LINE AND EXTENDED TO THE NORTHEASTERLY LINE OF TEMPLE TERRACE ESTATES ACCORDING TO MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 10 PAGE 67; THENCE SOUTHEASTERLY ALONG THE EASTERLY LINE OF SAID SUBDIVISION TO THE EASTERLY CORNER OF LOT 10 OF SAID SUBDIVISION; THENCE SOUTHWESTERLY ALONG THE SOUTHEASTERLY LINE OF LOTS 10 AND 7 OF SAID SUBDIVISION TO THE SOUTHERLY CORNER THEREOF; THENCE SOUTHEASTERLY ALONG THE SOUTHWESTERLY LINE OF LOT 8 OF SAID SUBDIVISION TO A POINT OF INTERSECTION WITH THE NORTHEASTERLY EXTENSION OF THE NORTHERLY LINE OF LOT 1, RIVER TERRACE ESTATES ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 53 PAGE 15; THENCE SOUTHWESTERLY ALONG SAID LINE TO THE NORTHWESTERLY CORNER THEREOF AND THE WATERS OF THE HILLSBOROUGH RIVER; THENCE MEANDERING NORTHWESTERLY ALONG THE WATERS OF THE HILLSBOROUGH RIVER TO THE AFOREDESCRIBED POINT OF BEGINNING AND BINDING THEREWITH TO.

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ORDINANCE NO. 1145

AN ORDINANCE OF THE CITY OF TEMPLE TERRACE, FLORIDA, AMENDING THE CITY'S COMPREHENSIVE PLAN AND MAP, BY CREATING A TRANSPORTATION CONCURRENCY EXCEPTION AREA (TCEA), AND RELATED POLICIES, APPLICABLE TO THE 225.31+/- ACRES IN THE CITY'S COMMUNITY REDEVELOPMENT AREA (CRA), AS SHOWN IN EXHIBIT "A" ATTACHED HERETO AND MADE A PART HEREOF, TO ENCOURAGE REDEVELOPMENT AND URBAN INFILL BY REDUCING TRANSPORTATION CONCURRENCY RESTRICTIONS THROUGH THE USE OF A RANGE OF TRANSPORTATION OPTIONS AND IMPROVED ACCESSIBILITY AND MOBILITY DESIGNS FOR NEW DEVELOPMENT, PURSUANT TO THE CITY'S REDEVELOPMENT MASTER PLAN; PROVIDING A DESCRIPTION AND DEFINITION RELATIVE TO THE NEW TRANSPORTATION CONCURRENCY EXCEPTION AREA (TCEA); BY AMENDING AND ENACTING OBJECTIVES AND POLICIES RELATIVE TO THE NEW TRANSPORTATION CONCURRENCY EXCEPTION AREA (TCEA); PROVIDING A SEPARABILITY CLAUSE, EFFECTIVE DATE, AND REPEALING ALL ORDINANCES OR PARTS OF ORDINANCES IN CONFLICT HEREWITH.

WHEREAS, the City of Temple Terrace Comprehensive Plan was adopted by Ordinance No. 694 on July 18, 1989; and

WHEREAS, the City Council has determined that an amendment to the City's Comprehensive Plan and Map is required to create a Transportation Concurrency Exception Area (TCEA), and related policies, applicable to the 225.31+/- acres in the City's Community Redevelopment Area (CRA) to encourage redevelopment and urban infill by reducing transportation concurrency restrictions through the use of a range of transportation options and improved accessibility and mobility designs for new development, pursuant to the City's redevelopment master plan; and

WHEREAS, this amendment is consistent with the Future Land Use Element – Downtown Redevelopment Goal 3 of the adopted Comprehensive Plan, and with proposed modifications of objectives and policies it is also consistent with Goal 6 – Growth Management of the Future Land Use Element,

NOW, THEREFORE, BE IT ORDAINED BY THE MAYOR AND CITY COUNCIL OF THE CITY OF TEMPLE TERRACE, FLORIDA:

Section 1. The City's Comprehensive Future Land Use Element, adopted by Ordinance No. 694, is hereby amended by creating a Transportation Concurrency Exception Area (TCEA), and related policies, applicable to the 225.31+/- acres in the City's Community Redevelopment Area (CRA) to encourage redevelopment and urban infill by reducing transportation concurrency restrictions through the use of a range of transportation options and improved accessibility and mobility designs for new development, pursuant to the City's redevelopment master plan.

Section 2. The City's Comprehensive Future Land Use Element, adopted by Ordinance No. 694, is hereby amended by enactment of changes and additions to the *Relationship to Other Areas of Use and the Street System* portion of the Description of the Downtown Mixed Use-25 (DMU-25) land use category:

DOWNTOWN MIXED USE-25

Description:

Relationship to Other Areas of Use and the Street System:

(Amended)

- Strip development with separate individual driveway access for commercial uses to arterials should be discouraged for new development. Proposed commercial development plans for sites abutting an arterial or collector street must include internal vehicle connections from the subject development site to each adjacent site, where applicable.

(Added)

- Proposed developments shall provide retail and service uses at the street level to promote a pedestrian-oriented environment. Facades shall be visually penetrable at the street level. Drive-through service windows shall be limited to the side of buildings, or to the rear of mid-block buildings, and accessed via alleys provided they do not substantially disrupt pedestrian activity or surrounding uses. In addition, drive-through service windows shall be accessible by bicyclists.
- New developments shall provide no more than the minimum number of parking spaces required for the proposed land use by the underlying zoning district. The following shall also apply:
 - On-street parking spaces on the right-of-way between the two side lot lines of the site may be counted to satisfy the minimum off-street parking requirements;
 - A commitment to shared parking may be allowed to further reduce the parking requirement;
 - Carpool/vanpool parking: New developments with 20 or more employees shall designate at least 5 percent of the employee spaces for carpool or vanpool parking. Employee carpool/vanpool spaces shall be located closer to the building entrance or the employee entrance than other employee parking with the exception of handicap parking. The carpool/vanpool spaces shall be clearly marked "Reserved – Carpool/Vanpool Only."

Section 3. The City's Comprehensive Plan, adopted by Ordinance No. 694, is hereby amended by enactment of the following *Definition* for the "Transportation Concurrency Exception Area" (TCEA) in the Definitions Section:

DEFINITIONS

Transportation Concurrency Exception Area (TCEA) – An area designated in the Plan, pursuant to Rule 9J-5.0055(6)(a)3., Florida Administrative Code, in which development may be exempt from transportation concurrency requirements as long as the impacts of development to the transportation system are mitigated using multi-modal strategies related to, but not limited to, such things as street connectivity, traffic calming, pedestrian facilities, pedestrian connectivity, bicycle facilities, and public transit service; and these strategies are provided for and specified in the Plan and approved by the State. In Temple Terrace, the TCEA encompasses the 225+/- acre downtown Community Redevelopment Area (CRA).

Section 4. The City's Comprehensive Future Land Use Element, adopted by Ordinance No. 694, is hereby amended by enacting a new Policy 3.1.2 in the Downtown Redevelopment section of the Goals, Objectives and Policies:

Policy 3.1.2:

Designate the Temple Terrace Community Redevelopment Area (CRA) as a Transportation Concurrency Exception Area (TCEA) for the purpose of encouraging urban infill and redevelopment of the City's downtown area consistent with Rule 9J-5.0055(6)(a)3., F.A.C.

Section 5. The City's Comprehensive Future Land Use Element, adopted by Ordinance No. 694, is hereby amended by repealing Policy 3.2.3 in the Downtown Redevelopment section of the Goals, Objectives and Policies and by enacting in its place a new Policy 3.2.3:

Policy 3.2.3:

Continue to utilize the City's Land Development Code to identify amenities such as bikeways, pedestrian walkways, landscaping, street trees, trash receptacles, benches, bike racks, information kiosks, bus shelters, lighting, signage, and pavement treatments, etc., to be provided in site development plans to improve and promote the pedestrian environment and support alternative modes of transportation in site designs, especially within the TCEA.

Section 6. The City's Comprehensive Future Land Use Element, adopted by Ordinance No. 694, is hereby amended by enacting a new Policy 3.2.9 and a new Policy 3.2.10 in the Downtown Redevelopment section of the Goals, Objectives and Policies:

Policy 3.2.9:

Development within the Temple Terrace Transportation Concurrency Exception Area (TCEA) may be exempt from transportation concurrency requirements as long as impacts are mitigated using the procedures set forth in Policies 1.1.10 and 1.1.11 of the Traffic Circulation and Mass Transit Element.

Policy 3.2.10:

Development or redevelopment within the TCEA shall include as part of design:

- a. Sidewalk connections from the developments to the public sidewalk;
- b. Cross-access connections, easements and/or joint driveways;
- c. Closure of existing overly wide or excessive curb cuts at the development site;
- d. Provision of safe and convenient on-site pedestrian circulation such as sidewalks and crosswalks connecting buildings and parking areas at the development site; and
- e. Provide for safe and convenient bicycle circulation and facilities.

Section 7: The City's Comprehensive Future Land Use Element, adopted by Ordinance No. 694, is hereby amended by enacting a new Objective 3.3 and Policies 3.3.1, 3.3.2 and 3.3.3 in the Downtown Redevelopment section of the Goals, Objectives and Policies:

Objective 3.3: The City shall promote multi-modal transportation choice.

Policy 3.3.1:

New streets and roadways shall be designed for transportation choice by creating design standards that establish minimum street widths, modest turning radii, modest design speeds, curb extensions, traffic calming, gridded and connected patterns, sidewalks, bicycle facilities and prohibition of cul-de-sacs and dead-end streets, where feasible.

Policy 3.3.2:

Design of surface parking lots within the Downtown Area, especially the DMU-25 designated areas of the downtown CRA, shall include design and access features which promote pedestrian, bicycle and transit safety and support pedestrian, bicycle and transit use in the area.

Policy 3.3.3:

Promote quality development that encourages a lively mixed use walkable downtown with emphasis on an interconnected network of streets that reweaves the downtown area into its surrounding context and provides new and improved connections with emphasis on an interconnected network of streets, expanded transit services and improved pedestrian and bicycle facilities that link the downtown to surrounding neighborhoods, communities, cultural attractions and regional destinations.

Section 8. The City's Comprehensive Future Land Use Element, adopted by Ordinance No. 694, is hereby amended by repealing Objective 6.1 for General Development in the Growth Management section of the Goals, Objectives and Policies and enacting in its place a new Objective 6.1:

Objective 6.1: Manage the City's land use by approving new development and redevelopment only if the public facilities are available at the adopted level of service standard established in the Public Facilities and Capital Improvements Elements of the Plan or if it lies within the designated Temple Terrace TCEA.

Section 9. The City's Comprehensive Future Land Use Element, adopted by Ordinance No. 694, is hereby amended by enacting a new Policy 6.3.3 in the Provision of Public Facilities – General Considerations section of the Goals, Objectives and Policies:

Policy 6.3.3:

Development within the Temple Terrace Transportation Concurrency Exception Area (TCEA) may be exempt from meeting the adopted level of service established in the Capital Improvement Element as long as impacts are mitigated using the procedures set forth in Policies 1.1.10 and 1.1.11 of the Traffic Circulation and Mass Transit Element.

Section 10. The City's Comprehensive Future Land Use Element, adopted by Ordinance No. 694, is hereby amended by repealing Policy 6.4.1 in the Provision of Public Facilities – Transportation section of the Goals, Objectives and Policies and enacting a new Policy 6.4.1 and a new Policy 6.4.5:

Policy 6.4.1:

Maximize arterial capacity by acquiring and/or protecting adequate rights-of-way, but place as much emphasis on arterial character as arterial capacity, especially within or adjacent to the downtown CRA. Rights-of-way acquisition for increased capacity should be balanced by efforts to enhance the interconnectedness of the overall street network.

Policy 6.4.5:

Development within the Temple Terrace Transportation Concurrency Exception Area (TCEA) may be exempt from meeting the adopted level of service established in the Capital Improvements Element as long as impacts are mitigated using the procedures set forth in Policies 1.1.10 and 1.1.11 of the Traffic Circulation and Mass Transit Element.

Section 11. The City's Comprehensive Traffic Circulation and Mass Transit Element, adopted by Ordinance No. 694, is hereby amended by enacting Policies 1.1.10, 1.1.11, and 1.1.12 within the Goals, Objectives and Policies:

Policy 1.1.10:

Pursuant to Rule 9J-5.0055(6)(a)3., F.A.C. and the Downtown Redevelopment Goal contained in the Future Land Use Element, the Temple Terrace Transportation Concurrency Area (TCEA) is hereby designated. (See Exhibit "A") Development within the Temple Terrace TCEA may be exempt from transportation concurrency requirements so long as the impacts to the transportation system are mitigated using the procedures below.

1. Any proposed development within the TCEA that would reduce the LOS on Florida Intrastate Highway System (FIHS) roadways within the City by more than five percent of the capacity of the adopted level of service standard must meet the transportation concurrency requirements specified in Rule 9J-5.0055(3)(c)1-4, F.A.C.
2. Any proposed development within the TCEA that would reduce the LOS on Florida Intrastate Highway System (FIHS) roadways within the City by less than five percent of the capacity at the adopted level of service standard and meets the requirements below in Policy 1.1.11 are exempt from the transportation concurrency requirements specified in Rule 9J-5.0055(3)(c)1-4, F.A.C.

Policies 1.1.10 (1) and (2) are included to ensure that the TCEA includes consideration of the impacts on the Florida Intrastate Highway System, as defined in s. 338.001. With I-75 being the closest FIHS facility to the TCEA, impacts to the FIHS are not anticipated.

Policy 1.1.11:

Developments within the Temple Terrace TCEA may obtain an exception from concurrency requirements provided that the following standards are met:

a. **Street Connectivity**

The street network within the TCEA and connecting the TCEA to immediately adjacent districts shall be comprised of a system of interconnected and direct routes with a connectivity index of 50 or more polygons (land areas bounded by streets) per square mile as measured in the *Multimodal Transportation Districts and Areawide Quality of Service Handbook* (FDOT, 2004 or latest edition). Missing links in the street network shall be identified and eliminated where feasible through the development and capital improvement processes.

b. **Traffic Calming**

While maximizing property access and providing a variety of direct routes, the network of local and collector streets shall discourage cut-through traffic from traveling into residential plan category areas through designs such as curving roads and jogs, T-intersections, roundabouts, gateway treatments, chicanes, speed tables, raised intersections, on-street parking, etc.

c. **Pedestrian Facilities**

A minimum of 80% of the pedestrian facilities in the TCEA shall function at LOS "C" or better, as defined in *Quality/Level of Service Handbook* (FDOT 2002 or latest edition). All roadways in the TCEA are assumed to be pedestrian facilities. Factors determining pedestrian LOS include: motor vehicle traffic volume and speed; presence of a sidewalk; and separation between vehicle traffic and pedestrians, such as with a grass strip, plantings, swales, wide outside lanes, or on-street parking.

d. **Pedestrian Connectivity**

The pedestrian network within the TCEA and connecting the TCEA to immediately adjacent districts shall be comprised of a system of interconnected and direct routes with a connectivity index of 50 or more polygons (land areas bounded by pedestrian ways) per square mile as measured in the *Multimodal Transportation Districts and Areawide Quality of Service Handbook* (FDOT, 2004 or latest edition). Missing links in the pedestrian network shall be identified and eliminated where feasible through the development and capital improvement processes. The maximum block length, without a pedestrian connection, shall be 400 feet.

e. **Bicycle Facilities**

A minimum of 80% of the bicycle facilities in the TCEA shall function at LOS "C" or better, as defined in *Quality/Level of Service Handbook* (FDOT 2002). All roadways in the TCEA are assumed to be bicycle facilities. Factors determining bicycle LOS include: motor vehicle speed, volume, and percent trucks; pavement width and surface condition; and the presence of a designated bike lane or paved shoulder.

f. **Public Transit Service**

Public transit service at a level of service "D" or better shall be provided at least in the arterial roadway corridors in the TCEA. Transit level of service, as defined in *Quality/Level of Service Handbook* (FDOT 2002 or latest edition), is determined by frequency, hours of service per day, and pedestrian access to stops, including ease of crossing the street. In addition, the following guidelines shall apply:

- All parcels within ¼ mile of a transit stop shall be served by pedestrian facilities at LOS "C" or better;
- 80% of employees and dwelling units in the TCEA shall be located within ½ mile of a transit stop.

Temple Terrace shall establish multi-modal street cross-sections, design standards, and operational measures (for example, pre-emptive signals, dedicated bus lanes, etc.) to ensure streets are safe, convenient, and appealing for all modes of travel, including transit, automobiles, trucks, bicycles, and pedestrians. Strategies shall include marked crosswalks, wider sidewalks, on-street parking, bus turnouts, traffic calming, raised medians, adequate drainage or other appropriate safety enhancements that reduce hazardous conflicts between modes and that are consistent with the planned functions of the roadway.

New developments or redevelopment projects shall contribute to providing a safe, convenient, comfortable and aesthetically pleasing transportation environment that promotes walking, cycling, and transit use. Appropriate improvements or enhancements to the multi-modal network may be required as a condition of development approval, and may include:

- Full accommodations for through streets, providing connectivity to adjacent roadways and properties, and traffic calming measures as appropriate;
- Full accommodations for pedestrian access and movement, including shaded sidewalks, benches and enhanced crossings;
- Full accommodations for bicycles, including lockers, showers, and racks;
- Direct connections to the regional bicycle and off-road trail network;
- Full accommodation for the mobility impaired, including parking spaces, sidewalks with clearly-edged curb cuts, and ramps for handicapped access;
- Weather protection at transit stops;
- Well-designed accommodations for transfer of passengers at designated transit facilities;
- Well-designed access for motor vehicle passenger drop-offs and pick-ups at designated transit facilities and at major residential and commercial development sites;
- Proportionate contribution to a City-administered fund supporting transit operations on specific routes serving the TCEA.

Policy 1.1.12:

The City shall amend the Concurrency Management section and any other relevant sections of the Land Development Code to reflect the adoption of the Transportation Concurrency Exception Area.

Section 12. The City's Comprehensive Traffic Circulation and Mass Transit Element, adopted by Ordinance No. 694, is hereby amended by repealing Policies 1.3.3, 1.4.2 and 1.4.3 enacting new Policies 1.3.3, 1.4.2, and 1.4.3 within the Goals, Objectives and Policies:

Policy 1.3.3:

New development shall provide land, fees, services, or facilities to mitigate the development's impact on the transportation system, except as may be provided pursuant to a TCEA or similar provision under State law.

Policy 1.4.2:

The City shall support the development of larger buffer areas to separate the arterial road network from residential development, except in the downtown CRA where urban design strategies shall seamlessly join higher density residential development to an appropriately scaled streetscape adjacent to the arterial road.

Policy 1.4.3:

The City shall consider the use of speed control methods such as speed restrictions, stop signs, cul-de-sacs, speed humps, street tree planning, on-street parking, or any combination so designed to provide a safe transportation network in residential neighborhoods.

Section 13. If any part of this ordinance is declared invalid by a court of competent jurisdiction, such part or parts shall be severable and the remaining part or parts hereof shall continue to be in full force and effect.

Section 14. This ordinance shall take effect immediately upon its passage, approval, and being posted or published as required by law.

Section 15. All ordinances or parts of ordinances not specifically in conflict herewith are hereby continued in full force and effect, but all ordinances or parts of ordinances in conflict herewith are hereby repealed.

PASSED AND ORDAINED BY THE CITY COUNCIL OF THE CITY OF TEMPLE TERRACE,
FLORIDA, THIS 17th DAY OF May, 2005.

APPROVED BY THE MAYOR THIS 17th DAY OF May, 2005.



Joseph A. Affronti, Sr.
JOSEPH A. AFFRONTI, SR., MAYOR

Melissa E. Burns
MELISSA E. BURNS, CMC
CITY CLERK

EXHIBIT "A"

CPA-04-10 – (Transportation Concurrency Exception Area) – Text Amendment

LEGAL DESCRIPTION:

BEING A PORTION OF SECTIONS 22 AND 27, TOWNSHIP 28 SOUTH, RANGE 19 EAST, TEMPLE TERRACE, HILLSBOROUGH COUNTY, FLORIDA.

BEGINNING AT THE SOUTHEASTERLY CORNER OF LOT 6A OF RIVERHILLS PARK UNIT NO. 1 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 30 PAGE 57 AND THE WATERS OF THE HILLSBOROUGH RIVER; THENCE NORTHERLY ALONG THE EASTERLY LINE OF SAID LOT 6A TO THE NORTHEASTERLY CORNER THEREOF TO A POINT ON THE SOUTHERLY RIGHT-OF-WAY LINE OF RIVERHILLS DRIVE; THENCE NORTHEASTERLY ALONG SAID LINE TO A POINT OF INTERSECTION WITH THE SOUTHERLY EXTENSION OF THE WESTERLY LOT LINE OF LOT 3 OF SAID RIVERHILLS PARK; THENCE NORTHERLY ALONG SAID EASTERLY LINE TO THE SOUTHWESTERLY LOT CORNER OF LOT 12, BLOCK 2 OF GRANDVIEW ACRES UNIT N. 1 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 32 PAGE 19; THENCE NORTHERLY ALONG THE WESTERLY LINE OF SAID LOT 12 TO THE NORTHWESTERLY CORNER THEREOF, ALSO BEING THE SOUTHWESTERLY CORNER OF LOT 2 BLOCK 2; THENCE NORTHERLY ALONG THE WESTERLY LINE OF SAID LOT 2 TO THE NORTHWESTERLY CORNER THEREOF; THENCE NORTHERLY TO A POINT OF INTERSECTION OF THE NORTHERLY RIGHT-OF-WAY LINE OF BEVERLY DRIVE AND THE SOUTHERLY EXTENSION OF THE EASTERLY LINE OF LOTS 4 AND 5 BLOCK 1 OF SAID GRANDVIEW ACRES UNIT NO. 1; THENCE NORTHERLY ALONG SAID LINE TO THE SOUTHEASTERLY CORNER OF LOT 6 OF SAID BLOCK 1; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF SAID LOT 6 AND EXTENDED TO THE INTERSECTION WITH THE WESTERLY RIGHT-OF-WAY LINE OF BEVERLY DRIVE; THENCE SOUTHERLY ALONG SAID LINE TO THE SOUTHEASTERLY CORNER OF LOT 7 BLOCK 5 OF SAID GRANDVIEW ACRES UNIT NO. 1; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF LOTS 7 THROUGH 1 OF SAID BLOCK 5 AND EXTENDED TO THE SOUTHEASTERLY CORNER OF LOT 1 BLOCK 7 OF SAID GRANDVIEW ACRES UNIT NO. 2 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 32 PAGE 38; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF LOTS 1 THROUGH 5 OF SAID BLOCK 7 TO THE SOUTHWESTERLY CORNER OF LOT 5 OF SAID BLOCK 7; THENCE NORTHERLY ALONG THE WESTERLY LINE OF SAID LOT 5 TO THE NORTHWESTERLY CORNER THEREOF; THENCE NORTHERLY TO THE SOUTHWESTERLY CORNER OF LOT 1 BLOCK 2 OF TERRACE OVERLOOK NO. 1 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 12 PAGE 36; THENCE NORTHERLY ALONG THE WESTERLY LINE OF LOTS 1 THROUGH 24 AND LOT 27 OF SAID BLOCK 2 TO THE NORTHWESTERLY CORNER OF SAID 27; THENCE NORTHERLY TO THE SOUTHWESTERLY CORNER OF LOT 27 BLOCK 4 OF TERRACE OVERLOOK NO. 2 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 12 PAGE 24; THENCE NORTHERLY ALONG THE WESTERLY LINE OF LOTS 27 AND 24 THROUGH 1 OF SAID BLOCK 4 TO THE NORTHWESTERLY CORNER OF SAID LOT 1; THENCE NORTHERLY TO THE SOUTHEASTERLY CORNER OF WASHINGTON SQUARE TOWNHOME CONDOMINIUMS ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN CONDOMINIUM PLAT BOOK 6, PAGE 34; THENCE NORTHERLY ALONG THE EASTERLY LINE OF SAID CONDOMINIUM TO THE NORTHEASTERLY CORNER THEREOF SAME ALSO BEING THE SOUTHWESTERLY CORNER OF UNIVERSITY SQUARE UNIT NO. 3 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 43 PAGE 82; THENCE ALONG THE SOUTHERLY LINE OF SAID UNIVERSITY SQUARE UNIT NO. 3 TO THE NORTHEASTERLY CORNER OF A PARCEL AS DESCRIBED IN O.R. BOOK 4443 PAGE 1642; THENCE SOUTHERLY ALONG THE EASTERLY LINE OF SAID O.R. BOOK 4443 TO THE NORTHWESTERLY CORNER OF A PARCEL AS DESCRIBED IN O.R. BOOK 8420 PAGE 1166; THENCE EASTERLY ALONG THE NORTHERLY LINE OF SAID

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O.R. BOOK 8420 TO THE NORTHEASTERLY CORNER THEREOF; THENCE SOUTHWESTERLY TO THE SOUTHWESTERLY CORNER OF A PARCEL AS DESCRIBED IN O.R. BOOK 3471 PAGE 185; THENCE EASTERLY ALONG THE SOUTHERLY LINE OF SAID O.R. BOOK 3471 TO THE SOUTHEASTERLY CORNER THEREOF SAID CORNER ALSO BEING THE SOUTHWESTERLY CORNER OF A PARCEL AS DESCRIBED IN O.R. BOOK 4521 PAGE 406; THENCE EASTERLY ALONG THE SOUTHERLY LINE OF SAID O.R. BOOK 4521 TO THE SOUTHEASTERLY CORNER THEREOF; THENCE EASTERLY TO THE SOUTHWESTERLY CORNER OF A PARCEL AS DESCRIBED IN O.R. BOOK 4602 PAGE 1952; THENCE EASTERLY ALONG THE SOUTHERLY LINE OF SAID O.R. BOOK 4602 TO THE SOUTHEASTERLY CORNER THEREOF, SAID CORNER ALSO BEING ON THE WESTERLY BOUNDARY OF TEMPLE HEIGHTS ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 37 PAGE 13; THENCE SOUTHERLY ALONG SAID LINE TO THE SOUTHWESTERLY CORNER OF LOT 7 OF SAID TEMPLE HEIGHTS; THENCE EASTERLY ALONG THE SOUTHERLY LINE OF SAID LOT 7 AND EXTENDED TO THE EASTERLY LINE OF SAID TEMPLE HEIGHTS; THENCE NORTHERLY ALONG SAID LINE TO THE NORTHEASTERLY CORNER THEREOF SAID CORNER ALSO BEING ON THE SOUTHERLY LINE OF TEMPLE TERRACE PLAZA ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 33 PAGE 42; THENCE WESTERLY ALONG SAID LINE TO SOUTHEASTERLY CORNER OF LOT 14 OF SAID TEMPLE TERRACE PLAZA; THENCE NORTHERLY ALONG THE EASTERLY LINE OF SAID LOT 14 TO THE NORTHEASTERLY CORNER THEREOF; THENCE EASTERLY ALONG THE NORTHERLY LINE OF LOTS 15 THROUGH 18 OF SAID TEMPLE TERRACE PLAZA AND EXTENDED TO THE EASTERLY RIGHT-OF-WAY LINE OF 56TH STREET; THENCE NORTHERLY ALONG SAID LINE TO THE SOUTHWESTERLY CORNER OF LOT 1 BLOCK 5, TEMPLE TERRACE HEIGHTS ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 31 PAGE 76; THENCE EASTERLY ALONG THE SOUTHERN LINE OF SAID BLOCK 5 TO THE SOUTHEASTERLY CORNER OF SAID BLOCK 5; THENCE SOUTHERLY ALONG THE WESTERLY RIGHT-OF-WAY LINE OF RACHEL STREET AND LOT 1 BLOCK 4 TO THE SOUTHWESTERLY CORNER OF SAID BLOCK 4; THENCE EASTERLY ALONG THE SOUTHERLY LINE OF BLOCK 4 TO THE SOUTHEASTERLY CORNER THEREOF SAID CORNER ALSO BEING ON THE WESTERLY RIGHT-OF-WAY LINE OF RIDGEDALE ROAD AND ALSO THE INTERSECTION OF THE NORTH LINE OF LOT 7 OF TEMPLE TERRACE ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 25 PAGE 68; THENCE SOUTHERLY ALONG SAID LINE TO THE INTERSECTION OF THE SOUTHERLY LINE OF LOT 8 OF SAID TEMPLE TERRACE AND SAID WESTERLY RIGHT-OF-WAY LINE; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF SAID LOT 8 TO THE NORTHEASTERLY CORNER OF TEMPLE TERRACE ESTATES ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 23 PAGE 25; THENCE SOUTHERLY ALONG THE EASTERLY LINE OF SAID TEMPLE TERRACE ESTATES TO THE P.I. OF THE SOUTHEASTERLY CORNER OF BLOCK F-10 OF SAID TEMPLE TERRACE ESTATES; THENCE SOUTHEASTERLY TO THE NORTHWESTERLY CORNER OF LOT 20 BLOCK C-3 OF TEMPLE TERRACE ESTATES ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 10 PAGE 62; THENCE SOUTHERLY ALONG THE WESTERLY LINE OF SAID LOT 20 TO THE SOUTHWESTERLY CORNER THEREOF; THENCE WESTERLY ALONG THE NORTHERLY LINE OF LOT 22 OF SAID BLOCK C-3 TO THE NORTHWESTERLY CORNER OF SAID LOT 22; THENCE SOUTHERLY ALONG THE WESTERLY LINE OF LOTS 22 THROUGH 25 OF SAID BLOCK C-3 TO THE SOUTHEASTERLY CORNER OF LOT 13 OF SAID BLOCK C-3; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF SAID LOT 13 AND EXTENDED TO THE WESTERLY RIGHT-OF-WAY LINE OF SUNNYSIDE ROAD AND THE EASTERLY LINE OF BLOCK D-8 OF TEMPLE TERRACE ESTATES ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 17 PAGE 22; THENCE SOUTHERLY ALONG SAID LINE TO THE SOUTHEASTERLY CORNER OF SAID BLOCK D-8 AND THE NORTHERLY RIGHT-OF-WAY LINE OF SHADOWLAWN AVENUE; THENCE SOUTHWESTERLY ALONG SAID LINE AND EXTENDED TO THE EASTERLY LINE OF A PARCEL AS DESCRIBED IN O.R. 8237 BOOK 1234; THENCE SOUTHERLY ALONG SAID LINE AND EXTENDED TO THE SOUTHERLY RIGHT-OF-WAY LINE OF CHICAGO AVENUE; THENCE

SOUTHERLY ALONG SAID LINE AND EXTENDED TO THE NORTHEASTERLY LINE OF TEMPLE TERRACE ESTATES ACCORDING TO MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 10 PAGE 67; THENCE SOUTHEASTERLY ALONG THE EASTERLY LINE OF SAID SUBDIVISION TO THE EASTERLY CORNER OF LOT 10 OF SAID SUBDIVISION; THENCE SOUTHWESTERLY ALONG THE SOUTHEASTERLY LINE OF LOTS 10 AND 7 OF SAID SUBDIVISION TO THE SOUTHERLY CORNER THEREOF; THENCE SOUTHEASTERLY ALONG THE SOUTHWESTERLY LINE OF LOT 8 OF SAID SUBDIVISION TO A POINT OF INTERSECTION WITH THE NORTHEASTERLY EXTENSION OF THE NORTHERLY LINE OF LOT 1, RIVER TERRACE ESTATES ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 53 PAGE 15; THENCE SOUTHWESTERLY ALONG SAID LINE TO THE NORTHWESTERLY CORNER THEREOF AND THE WATERS OF THE HILLSBOROUGH RIVER; THENCE MEANDERING NORTHWESTERLY ALONG THE WATERS OF THE HILLSBOROUGH RIVER TO THE AFOREDESCRIBED POINT OF BEGINNING AND BINDING THEREWITH TO.

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TEMPLE TERRACE CODE
CHAPTER 29 - TEMPLE TERRACE DOWNTOWN COMMUNITY REDEVELOPMENT PLAN
OVERLAY ZONING DISTRICT

Section 29.900 **TITLE.**

This Chapter shall be known and may be cited as the “**Temple Terrace Downtown Community Redevelopment Plan Overlay Zoning District.**”

Section 29.905 **PURPOSE AND INTENT.**

The purpose of this Chapter is to:

(a) Implement the Temple Terrace Downtown Redevelopment Plan (Adopted May 15, 2001), and the Downtown Temple Terrace Revitalization Master Plan (January 31, 2005), which establish the basis for the City’s vision of the downtown.

(b) Implement the Downtown Mixed Use District (DMU-25) comprehensive plan future land use category and Transportation Concurrency Exception Area.

(c) Recognize the dependency of commercial buildings and development on having high visibility from the public street. Therefore, the design of building(s) and sites determines much of the image and attractiveness of the streetscapes and character of a community. Massive and/or generic developments that do not contribute to, or integrate with, the community in a positive manner can be detrimental to a community’s image, and sense of place. The goal is to create and maintain a positive ambiance, pedestrian-oriented scale of development, and strong community image and identity by providing for architectural and site design standards that will enhance the visual appearance of commercial development within the Temple Terrace Downtown Community Redevelopment Area, as well as the City of Temple Terrace as a whole, while still providing for design flexibility. These standards are intended to enhance the quality of life in the City of Temple Terrace.

(d) Establish design guidelines and development standards for the Temple Terrace Downtown Redevelopment Plan Area that:

- (1) Ensure that the design and development of the public realm and private property are coordinated and tailored to provide high quality development.
- (2) Ensure that buildings that face the public realm have architectural designs with detailed façade features to create a pedestrian-scale streetscape and orient parking and service functions to the rear of buildings and interior of blocks.
- (3) Create a mixture of uses to include shops, workplaces, residences and civic buildings.
- (4) Ensure a design and scale transition of uses and building scale from the Downtown core to the adjacent established residential neighborhoods.

(e) Establish a process for the review of all development within the Temple Terrace Downtown Redevelopment Plan Area to evaluate compliance with the provisions of this Chapter.

Section 29.910 **TEMPLE TERRACE DOWNTOWN COMMUNITY REDEVELOPMENT PLAN AREA.**

(a) The area referred to as the “Temple Terrace Downtown Community Redevelopment Plan Area ” consists of approximately 225.31 acres that is bounded on the south by the Hillsborough River; on the west by the City limits (less the residential property fronting on Rainbow Drive, Shirley Drive, Grandview Drive, Ridgeway Drive, Riverhills Drive and Beverly Drive south of Grove Hill Road; on the north by the properties fronting on Temple Heights Road to Beverly Drive and the property south of 98th Avenue/Linda Avenue (extended); and on the east by Ridgedale Road, Sunnyside Road, Shadow Lane and Springdale Place. A map and legal description of the Temple Terrace Downtown Redevelopment Plan Area is found in Section 29.935 (a) Exhibit I of this Chapter.

TEMPLE TERRACE CODE
CHAPTER 29 - TEMPLE TERRACE DOWNTOWN COMMUNITY REDEVELOPMENT PLAN
OVERLAY ZONING DISTRICT

Section 29.915 DEVELOPMENT REVIEW PROCESS.

(a) All rezoning requests for property located within the Temple Terrace Downtown Community Redevelopment Plan Area shall be in the form of Planned Development (PD) District rezoning petitions, reviewed and processed pursuant to Chapter 25, Section 25.530.11 and other applicable provisions of Chapter 25.

(b) All development plans and/or permits shall be reviewed by the City's Development Review Committee (DRC) for compliance with the guidelines and standards in this Chapter. The DRC review shall be conducted pursuant to the procedures established in Chapter 25, Sections 25.325, 25.630 and all other Chapter 25 provisions not in conflict with this Chapter. The DRC may recommend approval, approval with conditions or denial of any development plans and/or permits reviewed pursuant to this Chapter.

(c) The DRC may recommend to the City Council the granting of waivers, for good cause shown, from the strict application of the design guidelines and standards of this chapter. In reviewing waiver requests from this Chapter, the DRC shall consider the overall intent, purpose and objectives of the Temple Terrace Downtown Redevelopment Plan, Downtown Temple Terrace Revitalization Master Plan, Downtown Mixed Use-25 future land use category and provisions of this Chapter in formulating recommendations.

(d) Final approval, approval with conditions or denial of all development plans and/or permits may be granted by City Council pursuant to the provisions of this Chapter and Chapter 25.

(e) In the event that there are any conflicts and/or inconsistencies between this Chapter and Chapter 25, the provisions of this Chapter shall prevail. If there is any dispute about which provisions shall prevail, such dispute shall be decided by the City Council.

Section 29.920 DRC APPEAL PROCESS.

(a) Decisions of the Development Review Committee (DRC) may be appealed to the City Council pursuant to the provisions of this Chapter and Chapter 25.

Section 29.925 DEVELOPMENT DESIGN GUIDELINES AND STANDARDS.

(a) All rezoning petitions, new developments and/or substantial expansion of existing developments on property located within the Temple Terrace Downtown Community Redevelopment Plan Area shall be consistent with the design guidelines and standards contained in this Chapter. A single-family dwelling unit existing at the time of the adoption of this Chapter is exempt from the requirements of this Chapter with the exception of Section 29.925.2 (d). Master plans or site plans approved by the City Council prior to the adoption of this Chapter are exempt, unless there is a modification that requires site plan review or the approval expires prior to issuance of a building permit.

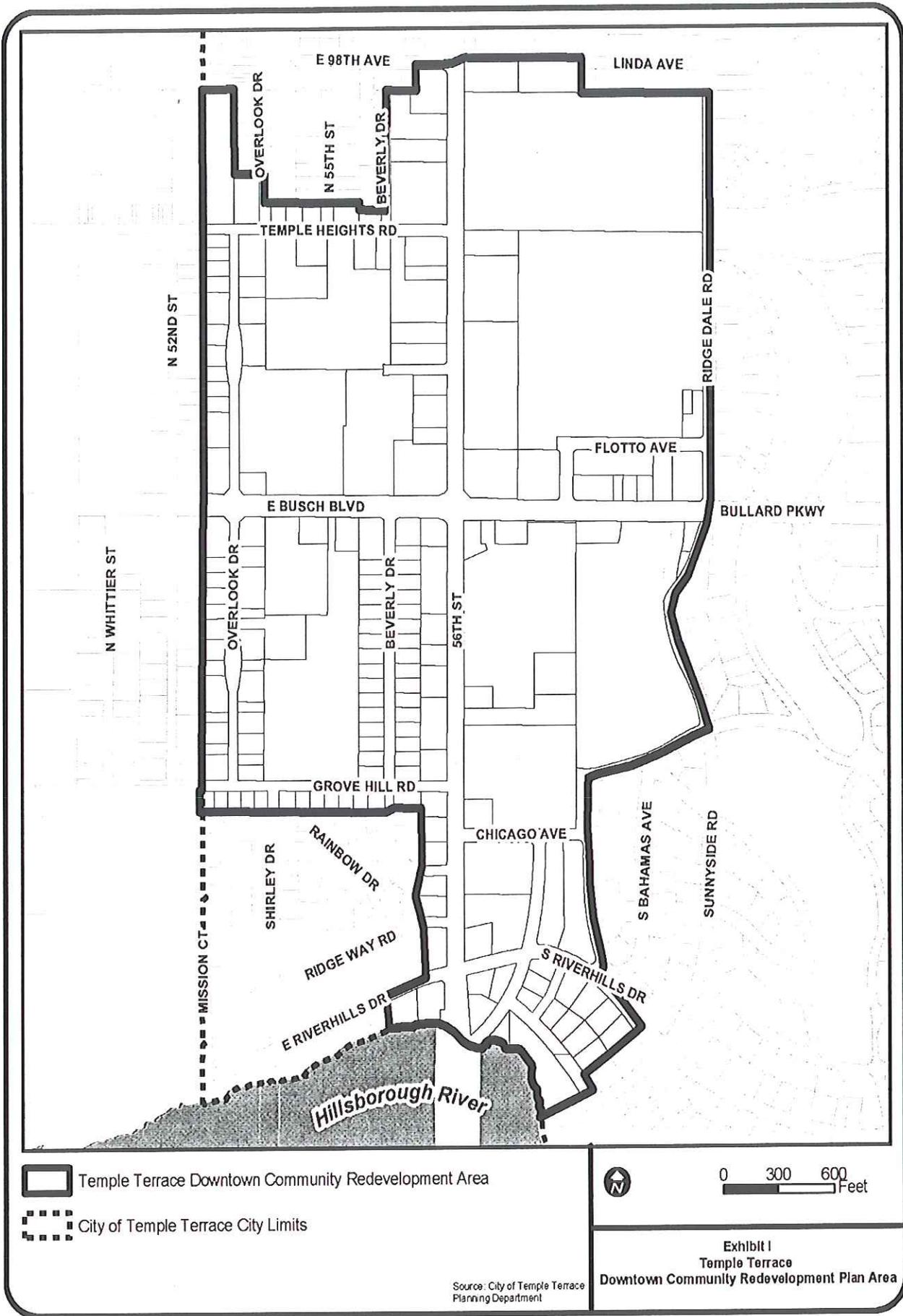
(b) Waivers to any design guidelines and standards shall only be approved by the City Council. A waiver may be granted provided the waiver is consistent with the intent and purpose of this Chapter and the City Council finds that:

- (1) There are extraordinary and exceptional conditions pertaining to the particular piece of property in question because of its size, shape or topography; and
- (2) The application of the provisions of this Chapter to the particular piece of property would create an unnecessary hardship; and

TEMPLE TERRACE CODE
CHAPTER 29 - TEMPLE TERRACE DOWNTOWN COMMUNITY REDEVELOPMENT PLAN
OVERLAY ZONING DISTRICT

- (3) Such conditions are peculiar to the particular piece of property involved; and
- (4) Relief, if granted, would not cause substantial detriment to the public good or impair the purposes and intent of this Chapter; and
- (5) Any waiver granted shall be the minimum waiver which makes possible the reasonable use of the land, building, or property.
- (6) Waivers may be granted to this Chapter for religious and educational institutions existing within the Temple Terrace Downtown Community Redevelopment Plan area at the time of the adoption of this Chapter notwithstanding the above criteria.

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 Temple Terrace Downtown Community Redevelopment Area

 City of Temple Terrace City Limits



0 300 600 Feet

Exhibit I
 Temple Terrace
 Downtown Community Redevelopment Plan Area

Source: City of Temple Terrace
 Planning Department

TEMPLE TERRACE CODE
CHAPTER 29 - TEMPLE TERRACE DOWNTOWN COMMUNITY REDEVELOPMENT PLAN
OVERLAY ZONING DISTRICT

Exhibit I
LEGAL DESCRIPTION

BEING A PORTION OF SECTIONS 22 AND 27, TOWNSHIP 28 SOUTH, RANGE 19 EAST, TEMPLE TERRACE, HILLSBOROUGH COUNTY, FLORIDA.

BEGINNING AT THE SOUTHEASTERLY CORNER OF LOT 6A OF RIVERHILLS PARK UNIT NO. 1 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 30 PAGE 57 AND THE WATERS OF THE HILLSBOROUGH RIVER; THENCE NORTHERLY ALONG THE EASTERLY LINE OF SAID LOT 6A TO THE NORTHEASTERLY CORNER THEREOF TO A POINT ON THE SOUTHERLY RIGHT-OF-WAY LINE OF RIVERHILLS DRIVE; THENCE NORTHEASTERLY ALONG SAID LINE TO A POINT OF INTERSECTION WITH THE SOUTHERLY EXTENSION OF THE WESTERLY LOT LINE OF LOT 3 OF SAID RIVERHILLS PARK; THENCE NORTHERLY ALONG SAID EASTERLY LINE TO THE SOUTHWESTERLY LOT CORNER OF LOT 12, BLOCK 2 OF GRANDVIEW ACRES UNIT N. 1 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 32 PAGE 19; THENCE NORTHERLY ALONG THE WESTERLY LINE OF SAID LOT 12 TO THE NORTHWESTERLY CORNER THEREOF, ALSO BEING THE SOUTHWESTERLY CORNER OF LOT 2 BLOCK 2; THENCE NORTHERLY ALONG THE WESTERLY LINE OF SAID LOT 2 TO THE NORTHWESTERLY CORNER THEREOF; THENCE NORTHERLY TO A POINT OF INTERSECTION OF THE NORTHERLY RIGHT-OF-WAY LINE OF BEVERLY DRIVE AND THE SOUTHERLY EXTENSION OF THE EASTERLY LINE OF LOTS 4 AND 5 BLOCK 1 OF SAID GRANDVIEW ACRES UNIT NO. 1; THENCE NORTHERLY ALONG SAID LINE TO THE SOUTHEASTERLY CORNER OF LOT 6 OF SAID BLOCK 1; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF SAID LOT 6 AND EXTENDED TO THE INTERSECTION WITH THE WESTERLY RIGHT-OF-WAY LINE OF BEVERLY DRIVE; THENCE SOUTHERLY ALONG SAID LINE TO THE SOUTHEASTERLY CORNER OF LOT 7 BLOCK 5 OF SAID GRANDVIEW ACRES UNIT NO. 1; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF LOTS 7 THROUGH 1 OF SAID BLOCK 5 AND EXTENDED TO THE SOUTHEASTERLY CORNER OF LOT 1 BLOCK 7 OF SAID GRANDVIEW ACRES UNIT NO. 2 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 32 PAGE 38; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF LOTS 1 THROUGH 5 OF SAID BLOCK 7 TO THE SOUTHWESTERLY CORNER OF LOT 5 OF SAID BLOCK 7; THENCE NORTHERLY ALONG THE WESTERLY LINE OF SAID LOT 5 TO THE NORTHWESTERLY CORNER THEREOF; THENCE NORTHERLY TO THE SOUTHWESTERLY CORNER OF LOT 1 BLOCK 2 OF TERRACE OVERLOOK NO. 1 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 12 PAGE 36; THENCE NORTHERLY ALONG THE WESTERLY LINE OF LOTS 1 THROUGH 24 AND LOT 27 OF SAID BLOCK 2 TO THE NORTHWESTERLY CORNER OF SAID 27; THENCE NORTHERLY TO THE SOUTHWESTERLY CORNER OF LOT 27 BLOCK 4 OF TERRACE OVERLOOK NO. 2 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 12 PAGE 24; THENCE NORTHERLY ALONG THE WESTERLY LINE OF LOTS 27 AND 24 THROUGH 1 OF SAID BLOCK 4 TO THE NORTHWESTERLY CORNER OF SAID LOT 1; THENCE NORTHERLY TO THE SOUTHEASTERLY CORNER OF WASHINGTON SQUARE TOWNHOME CONDOMINIUMS ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN CONDOMINIUM PLAT BOOK 6, PAGE 34; THENCE NORTHERLY ALONG THE EASTERLY LINE OF SAID CONDOMINIUM TO THE NORTHEASTERLY CORNER THEREOF SAME ALSO BEING THE SOUTHWESTERLY CORNER OF UNIVERSITY SQUARE UNIT NO. 3 ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 43 PAGE 82; THENCE ALONG THE SOUTHERLY LINE OF SAID UNIVERSITY SQUARE UNIT NO. 3 TO THE NORTHEASTERLY CORNER OF A PARCEL AS DESCRIBED IN O.R. BOOK 4443 PAGE 1642; THENCE SOUTHERLY ALONG THE EASTERLY LINE OF SAID O.R. BOOK 4443 TO THE NORTHWESTERLY CORNER OF A PARCEL AS DESCRIBED IN O.R. BOOK 8420 PAGE 1166; THENCE EASTERLY ALONG THE NORTHERLY LINE OF SAID O.R. BOOK 8420 TO THE NORTHEASTERLY CORNER THEREOF; THENCE SOUTHWESTERLY TO THE SOUTHWESTERLY CORNER OF A PARCEL AS DESCRIBED IN O.R. BOOK 3471 PAGE 185; THENCE EASTERLY ALONG THE

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TEMPLE TERRACE CODE
CHAPTER 29 - TEMPLE TERRACE DOWNTOWN COMMUNITY REDEVELOPMENT PLAN
OVERLAY ZONING DISTRICT

Exhibit I continued:

SOUTHERLY LINE OF SAID O.R. BOOK 3471 TO THE SOUTHEASTERLY CORNER THEREOF SAID CORNER ALSO BEING THE SOUTHWESTERLY CORNER OF A PARCEL AS DESCRIBED IN O.R. BOOK 4521 PAGE 406; THENCE EASTERLY ALONG THE SOUTHERLY LINE OF SAID O.R. BOOK 4521 TO THE SOUTHEASTERLY CORNER THEREOF; THENCE EASTERLY TO THE SOUTHWESTERLY CORNER OF A PARCEL AS DESCRIBED IN O.R. BOOK 4602 PAGE 1952; THENCE EASTERLY ALONG THE SOUTHERLY LINE OF SAID O.R. BOOK 4602 TO THE SOUTHEASTERLY CORNER THEREOF, SAID CORNER ALSO BEING ON THE WESTERLY BOUNDARY OF TEMPLE HEIGHTS ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 37 PAGE 13; THENCE SOUTHERLY ALONG SAID LINE TO THE SOUTHWESTERLY CORNER OF LOT 7 OF SAID TEMPLE HEIGHTS; THENCE EASTERLY ALONG THE SOUTHERLY LINE OF SAID LOT 7 AND EXTENDED TO THE EASTERLY LINE OF SAID TEMPLE HEIGHTS; THENCE NORTHERLY ALONG SAID LINE TO THE NORTHEASTERLY CORNER THEREOF SAID CORNER ALSO BEING ON THE SOUTHERLY LINE OF TEMPLE TERRACE PLAZA ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 33 PAGE 42; THENCE WESTERLY ALONG SAID LINE TO SOUTHEASTERLY CORNER OF LOT 14 OF SAID TEMPLE TERRACE PLAZA; THENCE NORTHERLY ALONG THE EASTERLY LINE OF SAID LOT 14 TO THE NORTHEASTERLY CORNER THEREOF; THENCE EASTERLY ALONG THE NORTHERLY LINE OF LOTS 15 THROUGH 18 OF SAID TEMPLE TERRACE PLAZA AND EXTENDED TO THE EASTERLY RIGHT-OF-WAY LINE OF 56TH STREET; THENCE NORTHERLY ALONG SAID LINE TO THE SOUTHWESTERLY CORNER OF LOT 1 BLOCK 5, TEMPLE TERRACE HEIGHTS ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 31 PAGE 76; THENCE EASTERLY ALONG THE SOUTHERN LINE OF SAID BLOCK 5 TO THE SOUTHEASTERLY CORNER OF SAID BLOCK 5; THENCE SOUTHERLY ALONG THE WESTERLY RIGHT-OF-WAY LINE OF RACHEL STREET AND LOT 1 BLOCK 4 TO THE SOUTHWESTERLY CORNER OF SAID BLOCK 4; THENCE EASTERLY ALONG THE SOUTHERLY LINE OF BLOCK 4 TO THE SOUTHEASTERLY CORNER THEREOF SAID CORNER ALSO BEING ON THE WESTERLY RIGHT-OF-WAY LINE OF RIDGEDALE ROAD AND ALSO THE INTERSECTION OF THE NORTH LINE OF LOT 7 OF TEMPLE TERRACE ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 25 PAGE 68; THENCE SOUTHERLY ALONG SAID LINE TO THE INTERSECTION OF THE SOUTHERLY LINE OF LOT 8 OF SAID TEMPLE TERRACE AND SAID WESTERLY RIGHT-OF-WAY LINE; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF SAID LOT 8 TO THE NORTHEASTERLY CORNER OF TEMPLE TERRACE ESTATES ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 23 PAGE 25; THENCE SOUTHERLY ALONG THE EASTERLY LINE OF SAID TEMPLE TERRACE ESTATES TO THE P.I. OF THE SOUTHEASTERLY CORNER OF BLOCK F-10 OF SAID TEMPLE TERRACE ESTATES; THENCE SOUTHEASTERLY TO THE NORTHWESTERLY CORNER OF LOT 20 BLOCK C-3 OF TEMPLE TERRACE ESTATES ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 10 PAGE 62; THENCE SOUTHERLY ALONG THE WESTERLY LINE OF SAID LOT 20 TO THE SOUTHWESTERLY CORNER THEREOF; THENCE WESTERLY ALONG THE NORTHERLY LINE OF LOT 22 OF SAID BLOCK C-3 TO THE NORTHWESTERLY CORNER OF SAID LOT 22; THENCE SOUTHERLY ALONG THE WESTERLY LINE OF LOTS 22 THROUGH 25 OF SAID BLOCK C-3 TO THE SOUTHEASTERLY CORNER OF LOT 13 OF SAID BLOCK C-3; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF SAID LOT 13 AND EXTENDED TO THE WESTERLY RIGHT-OF-WAY LINE OF SUNNYSIDE ROAD AND THE EASTERLY LINE OF BLOCK D-8 OF TEMPLE TERRACE ESTATES ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 17 PAGE 22; THENCE SOUTHERLY ALONG SAID LINE TO THE SOUTHEASTERLY CORNER OF SAID BLOCK D-8 AND THE NORTHERLY RIGHT-OF-WAY LINE OF SHADOWLAWN AVENUE; THENCE SOUTHWESTERLY ALONG SAID LINE AND EXTENDED TO THE EASTERLY LINE OF A PARCEL AS DESCRIBED IN O.R. 8237 BOOK 1234; THENCE SOUTHERLY ALONG SAID LINE AND EXTENDED TO THE SOUTHERLY RIGHT-OF-WAY LINE OF CHICAGO AVENUE; THENCE EASTERLY ALONG SAID LINE TO THE NORTHEASTERLY CORNER OF BLOCK D-14 OF TEMPLE TERRACE ESTATES ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 17 PAGE 22; SAME ALSO BEING THE WESTERLY RIGHT-OF-WAY LINE OF SPRING DELE PLACE; THENCE SOUTHERLY ALONG SAID LINE

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11-22-06

TEMPLE TERRACE CODE
CHAPTER 29 - TEMPLE TERRACE DOWNTOWN COMMUNITY REDEVELOPMENT PLAN
OVERLAY ZONING DISTRICT

Exhibit I continued:

AND EXTENDED TO THE NORTHEASTERLY LINE OF TEMPLE TERRACE ESTATES ACCORDING TO MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 10 PAGE 67; THENCE SOUTHEASTERLY ALONG THE EASTERLY LINE OF SAID SUBDIVISION TO THE EASTERLY CORNER OF LOT 10 OF SAID SUBDIVISION; THENCE SOUTHWESTERLY ALONG THE SOUTHEASTERLY LINE OF LOTS 10 AND 7 OF SAID SUBDIVISION TO THE SOUTHERLY CORNER THEREOF; THENCE SOUTHEASTERLY ALONG THE SOUTHWESTERLY LINE OF LOT 8 OF SAID SUBDIVISION TO A POINT OF INTERSECTION WITH THE NORTHEASTERLY EXTENSION OF THE NORTHERLY LINE OF LOT 1, RIVER TERRACE ESTATES ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 53 PAGE 15; THENCE SOUTHWESTERLY ALONG SAID LINE TO THE NORTHWESTERLY CORNER THEREOF AND THE WATERS OF THE HILLSBOROUGH RIVER; THENCE MEANDERING NORTHWESTERLY ALONG THE WATERS OF THE HILLSBOROUGH RIVER TO THE AFORE-DESCRIBED POINT OF BEGINNING AND BINDING THEREWITH TO.

TEMPLE TERRACE CODE
CHAPTER 29 - TEMPLE TERRACE DOWNTOWN COMMUNITY REDEVELOPMENT PLAN
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Section 29.925.1 BLOCK PERIMETER STANDARDS.

(a) There is a relationship between block size and pedestrian friendly design and orientation. The smaller the block size, the greater its pedestrian orientation in terms of short walking distances and building facade designs that are pedestrian in scale. A block perimeter is defined as a land area bounded by three (3) or more streets, but does not include alleys.

(b) Block perimeter standards for new unified development that encompasses a block area ensure that new developments will be connected to adjacent development along streets, and where appropriate along alleyways to enhance service and parking functions. Block perimeter standards are a function of the degree of pedestrian orientation desired for a particular street frontage. The Block Plan as shown in Exhibit II illustrates the intent of the block perimeter standards. However, individual parcel developments may require modifications to this plan to fit specific development block size requirements. The streets within the Downtown Community Redevelopment Area are classified in a pedestrian oriented hierarchy of most to least importance. The classifications are Retail Pedestrian, Primary and Secondary Streets (Exhibit V). The following block perimeter standards apply to these street classifications:

- (1) Retail Pedestrian Street: The maximum block perimeter- 1,600 linear feet or 2,250 linear feet if all parking is structured and located internal to the block.
- (2) Primary and Secondary Streets: The maximum block perimeter- 1,500 linear feet or 1,800 linear feet if all parking is structured and located internal to the block.

(c) Where new development creates new street networks, such new streets shall be aligned with the adjacent existing street network. At least two (2) of the new streets shall be classified as the highest street classification of the adjacent streets.

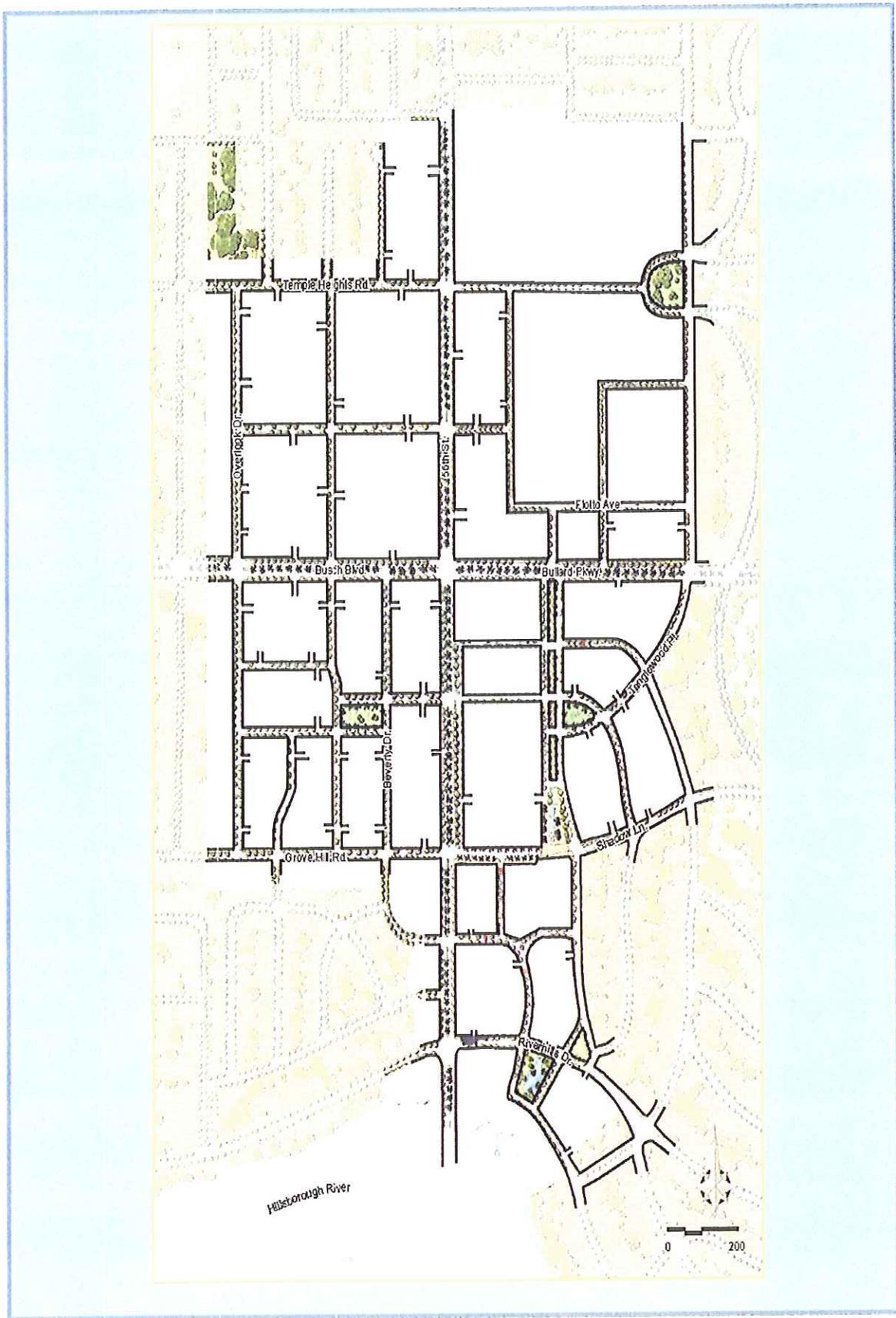


Exhibit II
Illustrative Block Perimeter Plan

TEMPLE TERRACE CODE
CHAPTER 29 - TEMPLE TERRACE DOWNTOWN COMMUNITY REDEVELOPMENT PLAN
OVERLAY ZONING DISTRICT

Section 29.925.2 ARCHITECTURAL STYLES GUIDELINES.

(a) New development and/or substantial expansion of existing development shall incorporate, at minimum, architectural design elements consistent with one of the following architectural styles: Spanish Renaissance Revival, Mediterranean Revival, or St. Augustine Spanish Colonial as defined and illustrated in Exhibits III-A.1-III-C.1 of this Chapter. Development shall include all of the following major design elements as illustrated in Exhibits III-A.1 - III-C.1:

- (1) Massing
- (2) Building facade materials and color
- (3) Roof form, details and materials
- (4) Window treatments
- (5) Building entrance features/details

(b) A development project with multiple buildings shall have a consistent architectural style and use of materials throughout the project.

(c) New development and/or substantial expansion of existing development shall include façade elements that provide shade along the street frontage such as awnings, canopies, arcades or colonnades and provide an additional design element related to the above referenced architectural styles, such as iron gates, tile fountains, cast terra cotta, wood work, stenciled ornaments, balconies or similar design features as part of a building façade, building frontage and/or zoning lot front yard.

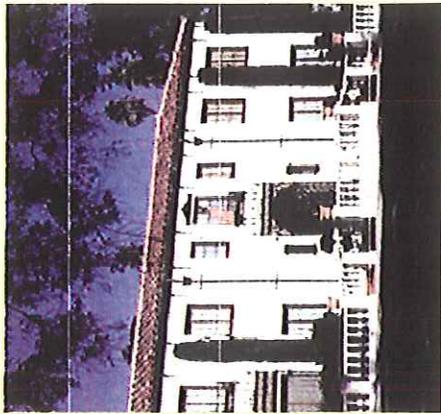
(d) The architectural style of additions that do not meet the definition of substantial expansion to an existing building shall be compatible with the existing building in design elements including, but not limited to style, form, height, materials, color, and details (Exhibit IV). An addition shall be located in a manner that maintains the integrity or prominence of the principal building. Where feasible, efforts shall be made to incorporate design concepts and elements consistent with the Spanish Renaissance Revival, Mediterranean Revival, or St. Augustine Spanish Colonial architectural styles on the principal building and addition.

Section 29.925.3 BUILDING FRONTAGE AND BUILD-TO-LINE STANDARD.

(a) Building frontage and build-to-line standards are based on the hierarchical classification of streets as Retail Pedestrian Street, Primary Street or Secondary Street, as shown on the Exhibit V.

(b) The design standards described below and illustrated in Exhibits VI, VII and VIII shall apply to all development along Retail Pedestrian Streets:

- (1) Building masses and façades along the street shall be articulated with windows, store fronts and entryways oriented to the sidewalk and the street. Variations in exterior building wall plane in depth or articulation (such as use of columns) is encouraged to avoid a monolithic appearance.
- (2) Buildings that are part of a unified development shall line along the street to create a continuous building frontage along each street. At least ninety (90) percent of the total street frontage shall have a continuous building frontage.



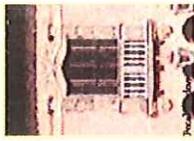
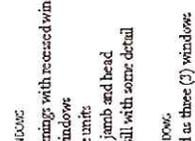
SPANISH RENAISSANCE REVIVAL - HIGH STYLE

The Temple Terrace Spanish Revival High Style is characterized by overall simplicity with detailed elements at key points such as building entries. Historically, these key points were detailed in a Spanish Baroque style (Churrigueresque) or Plateresque (16th Century Spanish/Moorish). The principal building entries shall be designed in a highly detailed manner. Thick walls with punched windows are also characteristic along with stone or precast sills with moderate detailing. This style is also characterized by red terracotta barrel tile roofs with a shallow pitch (4:12 or 5:12).

Some general characteristics of this style include:

- Simple forms, symmetrically arranged
- Hip roofs with a shallow pitch (4:12 or 5:12)
- Flat roof behind a parapet
- Red barrel tile roof
- Ornate entrance
- Massive stucco wall surface
- Arcades
- Focal windows
- Eaves with moderate overhangs
- Wall surfaces with less than 2.5% openings

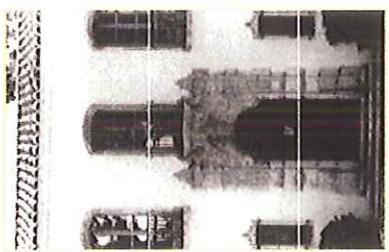
Windows

Typical Windows:
Rectangular punched window openings, with recessed windows
Casement windows
Multi-pane units
No trim around the jamb and head
No sill or stone/precast sill with some detail

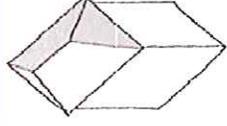
Foot Windows:
Single window or grouped as three (3) windows
Focal windows with balconet
Single balconies with "French" doors supported by brackets
Focal windows have stone or precast or plaster trim
Casement windows
Rectangular or arched windows

Doors



Recessed double door
Ornate principal entry doorway surrounded in stone or stucco
Arched or rectangular doorway
Window above doorway part of ornamentation

Massing




Hipped Roof
Gable with parapet

Simple forms (rectangle or box)
Hipped roof or Gable or Flat roof with parapet
Two (2) or three (3) stories
First floor taller than other floors (2:1)

Details

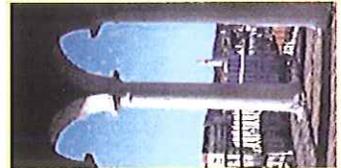
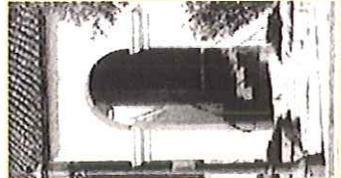





Moderate overhangs
Simple cornice
Absence of frieze or highly detailed frieze
Brackets under eave

Single balcony with brackets of stone/precast
Balcony railings of metal or stone/precast
Slightly undulating stucco

Arcades & Columns

Arched arcade supported by columns
Doric order
Deep arcades
Articulated pier or columns

Typical Materials

Cladding: Sand finish or slightly undulating stucco with stone/cast stone cornice or frieze

Roofing: Red barrel tile

Windows: Casement with true or faux divided lights

Trim: Plaster, stone/cast stone, or EIFS

Shutters: Not permitted

Gutters: None or invisibly integrated into the roof

Downspouts: Copper or other metal

Soffits: Sand finish stucco

Columns: Stone or cast stone

Railings: Ornamental metal or stone/precast ornamental rail or wood with ornamental metal proportions

Foundations: Stucco

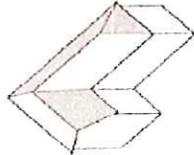
Fencing: Stucco walls

Colors: White, pale hue, or warm earth tones

*These materials may be substituted with other high quality material of similar appearance

Exhibit III-A.1 Spanish Revival Style

Massing



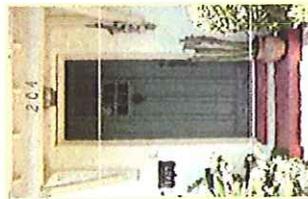
Hipped and/or gabled roof



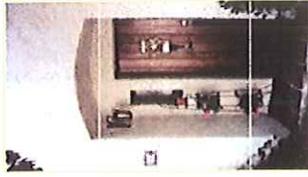
Hipped or gabled roof w/ Parapet

Simple rectangular forms or asymmetrical massing of additive simple forms
Hipped roof, gabled roof, or flat roof with parapet
Two (2) or three (3) stories

Doors



Recessed double door
Multi-paneled door
Decorative arched or trimmed entry door
Arched or rectangular doorway



Windows



Single balcony



Rectangular punched window openings with recessed windows
Casement windows or double-hung sash
Multi-pane units
Single windows or grouped as three (3) windows
Focal windows may be present
Single balconies with "French" doors supported by brackets
Stone or precast, stucco, or EIFS trim
Rectangular or arched windows

Typical Materials

- Cladding: Stucco or plaster
 - Roofing: Red barrel tile
 - Windows: Casement or double-hung sash with true or faux divided lights
 - Trim: Plaster, stone/cast stone, or EIFS
 - Shutters: Wood
 - Gutters: None or invisibly integrated into the roof
 - Downspouts: Copper or other metal
 - Soffits: Sand finish stucco
 - Columns: Stone, wood, cast stone, or precast concrete
 - Railings: Ornamental metal or stone/precast ornamental rail or wood with ornamental metal proportions
 - Foundations: Stucco
 - Fencing: Stucco walls
 - Colors: White, pale hues, or warm earth tones
- *These materials may be substituted with other high quality material of similar appearance

Arcades & Columns



Arcade supported by columns (round or square)
Deep arcades
Articulated piers or columns



Details



Small to moderate overhangs
Small overhang with cornice
Moderate overhang with brackets
Absence of frieze
Single balcony with brackets may be present
Balcony railings of metal, wood, or stone/precast
Garden walls w/ curve and transition

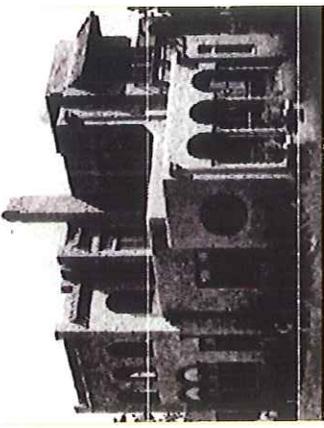
Exhibit III-A.2 Spanish Revival Style

SPANISH RENAISSANCE REVIVAL - VERNACULAR STYLE

The Temple Terrace Spanish Revival Vernacular Style is characterized by overall simplicity and asymmetrical massing. Thick walls with recessed windows, red terra-cotta barrel tile roofs with a shallow pitch (4:12 or 5:12), characterize the style.

Some general characteristics of this style include:

- Simple forms or asymmetrical massing
- Hip roofs with a shallow pitch and/or gable roofs (4:12 or 5:12)
- Gable roof behind a parapet
- Red barrel tile roof
- Decorated entry
- Massive stucco wall surface
- Arcades
- Focal windows
- Eaves with small to moderate overhangs
- Typically an absence of shutters





Villa Negroboni Girardinova, Italy

MEDITERRANEAN REVIVAL - HIGH STYLE

The Mediterranean Revival High Style in Temple Terrace is characterized by a very simple massing (usually rectangular) with red tile roof and white plaster or stucco walls. This style is a combination of Spanish, French, and Italian architecture with adaptation to local needs.

Some general characteristics of the Mediterranean Revival High Style include:

- Simple rectangular symmetrical massing and facade
- Hipped roof with shallow pitch (4:12 or 5:12)
- Central emphasis on primary facade with focal element
- Smooth stucco finish
- Red barrel tile roof
- Eaves with large overhangs
- Arched windows and doors as decorative accents
- Entry area usually accentuated
- Windows with vertical proportions
- Mouldings and details are neo-classical

Windows

Rectangular window openings with recessed windows
 Casement windows or double-hung sash
 Multi-pane units or faux divided lights
 Single windows or grouped as three (3) windows
 Upper-story windows may be smaller and less elaborate than windows below
 Working shutters

Doors

Recessed double door in vestibule or arcade
 Heavy wood door
 Multi-paneled door
 Decorative entry door with trim

Massing

Low Pitch-Hipped Roof

Simple forms
 Horizontal massing
 Symmetrical massing

Details

Moderate to large overhangs
 Wrought iron grilles and railings
 Wooden brackets and/or eaves
 Smooth stucco or plaster
 Some ornamentation
 Mouldings and details are neo-classical

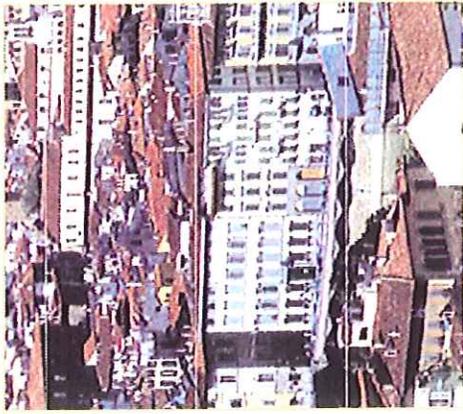
Arcades & Columns

Columns and pier proportions are neo-classical
 Arched openings
 Stone or cast stone columns

Typical Materials*

Cladding: Smooth stucco
 Roofing: Red barrel tile
 Windows: Casement or double-hung sash with divided lights
 Trim: Plaster, stone/cast stone, stucco, or EIRS
 Shutters: Wood or none
 Gutters: None or invisibly integrated into the roof
 Downspouts: Metal
 Soffits: Smooth stucco
 Columns: Stone, cast stone, or stucco
 Railings: Ornamental metal or stone/precast ornamental rail
 Foundations: Stucco
 Fencing: Stucco walls or stucco combined with ornamental wrought iron or steel
 Colors: White, pale blues, or warm earth tones
 *These materials may be substituted with other high quality material of similar appearance

Exhibit III-B.1 Mediterranean Revival Style



MEDITERRANEAN REVIVAL - VERNACULAR STYLE

The Temple Terrace Mediterranean Revival Vernacular Style is more informal and less ornate than the Mediterranean High Style. The massing is symmetrical or asymmetrical and covered by a red tile hipped roof.

Some general characteristics of the Mediterranean Revival Vernacular Style include:

- Hipped roof with shallow pitch or gable roof (4:12 or 5:12)
- Small single balconies
- May include square tower or other focal element
- Stucco finish
- Red barrel tile roof
- Eaves with moderate to large overhangs
- Ornamental metal, wood, or precast railings
- Arched windows and doors as decorative accents
- Wooden shutters or absence of shutters

Windows

Rectangular window openings with recessed windows
 Casement windows or double-hung sash
 Multi-pane units or fan divided lights
 Windows may have trim
 Single windows or grouped as three (3) windows
 Formal or less formal arrangement of windows

Details

Moderate to large overhangs
 Wooden brackets under eaves
 Plain or undulating stucco or plaster

Doors

Recessed double or single door
 Heavy wood door
 Multi-paned door
 Decorative entry door may have trim

Arcades & Columns

Arched openings
 Arched covered entry
 Half-round or pointed arches

Massing

Combination massing
 Simple or asymmetrical additive forms
 Low pitch, hipped roof can be combined w/ flat roof.

Typical Materials

Cladding: Smooth stucco
 Roofing: Red barrel tile
 Windows: Casement or double-hung sash with divided lights
 Trim: Plaster, stone/cast stone, stucco, or EIFS
 Shutters: Wood
 Gutters: None or invisibly integrated into the roof
 Downspouts: Metal
 Soffits: Sand finish stucco
 Columns: Stone, cast stone, or stucco
 Railings: Ornamental metal, wood, or stone/precast ornamental rail
 Foundations: Stucco
 Fencing: Stucco walls and/or combined with wrought iron
 Colors: White, pale hues, or warm earth tones

*These materials may be substituted with other high-quality material of similar appearance

**Exhibit III-B.2
 Mediterranean Revival Style**



SAINT AUGUSTINE SPANISH COLONIAL STYLE

The Saint Augustine style is a revival of the Spanish Colonial and the Anglo-influence of the northern part of Florida.

Some general characteristics of the St. Augustine Spanish Colonial Style include:

- Simple forms
- Pitched gable roof or hipped roof (10:12 or 12:12)
- Two (2) or three (3) stories with horizontal massing
- Stucco wall surface or stucco first floor and wood siding second story
- Stucco garden walls with doors
- Absence of light decorative elements
- Vertical oriented openings regularly spaced
- Cantilevered second floor balcony (or with supporting columns) with linear detailing
- Second floor balcony often covered by principal roof
- Wood rail, post, and balcony details

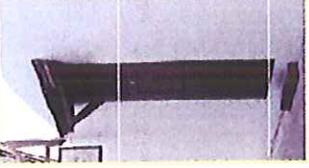
Windows



Window Detail

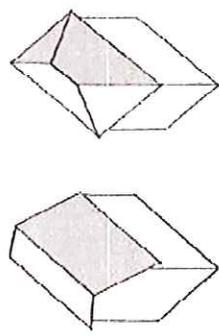
Rectangular window openings with recessed windows
Vertical proportions
Double-hung sash
Multi-pane units
Trim board at jamb and head
Sill or abacus of sill
Single windows

Doors



Recessed single or double door
Heavy wood door
Punctured door
Rectangular doorway
Somewhat entries with clad roof
Arched light, or garden threshold

Massing



Hipped Roof

Hybrid Gable Roof

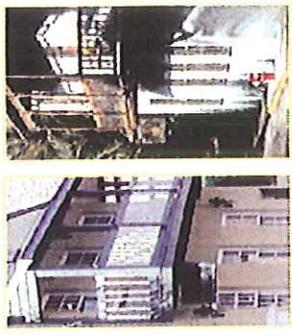
Simple forms
Pitched gable roof
Two (2) or three (3) stories - horizontal massing

Details



No gable overhang
Single cantilevered balcony
Simple undecorated wooden posts and railings
Wooden shutters
Wall mounted (colonial) light fixture

Arcades & Columns



Paris board and rail board
Simple wood railings
Simple wood columns

Typical Materials

Cladding: Smooth stucco, smooth plastered walls, wood siding, or stucco first floor and siding second floor

Roofing: Shingles

Windows: Double hung divided light windows

Trim: None

Shutters: Wood or none present

Gutters: None or invisibly integrated into the roof

Downspouts: Metal

Soffits: Sand finish stucco

Columns: Wood posts

Railings: Wood

Foundations: Stucco

Fencing: Stucco walls

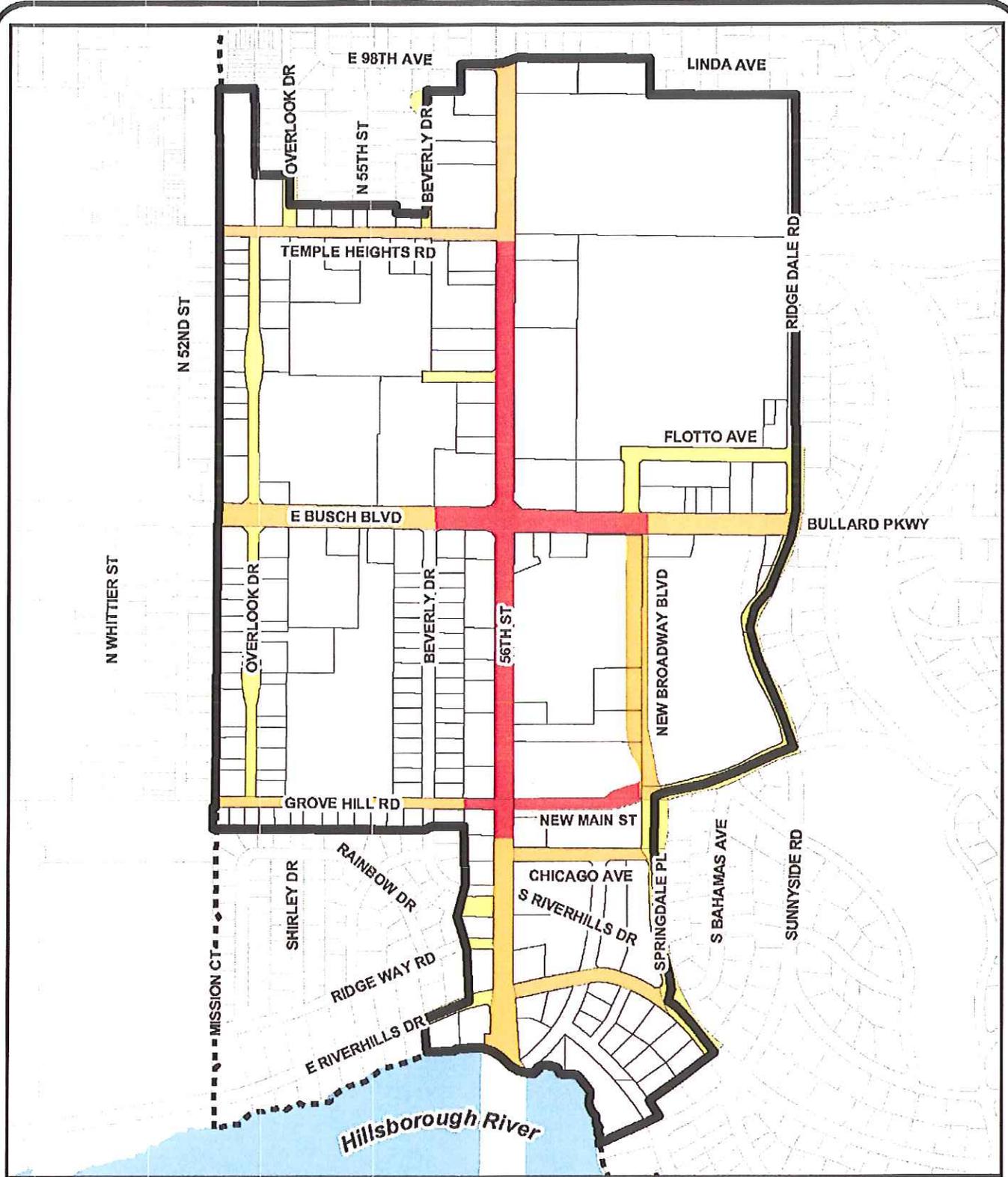
Colors: White, pale hues, or warm earth tones

**These materials may be substituted with other high quality material of similar appearance*

Exhibit III-C.1 Saint Augustine Spanish Colonial Style

TEMPLE TERRACE CODE
CHAPTER 29 - TEMPLE TERRACE DOWNTOWN COMMUNITY REDEVELOPMENT PLAN
OVERLAY ZONING DISTRICT

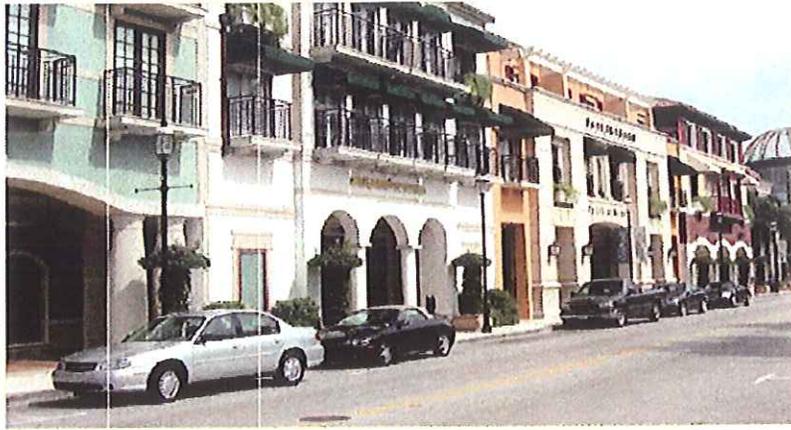
- (3) A building that is on a single parcel and not defined a part of a unified development is encouraged to create a continuous building frontage with the adjacent parcels by having zero (0) side yard setbacks (Exhibit VIII). However, such building may have side yard setbacks of not greater than five (5) feet from the property line, except as provide for in Section 25.925.6- Parking Lots and Garages and Loading Areas.
- (4) The distinction between ground floor level and upper floor levels shall be clearly articulated.
- (5) The ground level façade shall have architectural fenestration that creates a pedestrian-scale streetscape. The minimum ground floor ceiling height shall be eighteen (18) feet. The minimum building frontage height shall be two (2) stories or thirty (30) feet. The ground level façade shall have a minimum (70) percent of its total area in transparent glazing up to a height of sixteen (16) feet. The maximum percentage of glazed area that may be blocked by interior fixtures or signage along any street front is thirty (30) percent.
- (6) All retail development shall be at the ground floor level. One hundred (100) percent of the ground floor building frontage shall be designed for and occupied with retail uses.
- (7) Building frontage design elements shall include the use of awnings, arcades or colonnades, or similar features to provide pedestrian comfort zones. Arcades or colonnades may be located at the property line.
- (8) Blank building walls are prohibited on the street frontage and where side or rear walls of buildings are visible from the street. Where side and rear walls are not visible from the street blank walls are discouraged.
- (9) Buildings shall meet the build-to-line, which supersedes any zoning district front or street side yard setback requirement. The build-to-line shall be ten (10) feet from the property line adjacent to right-of-way.
- (10) The pavement finish (for example, patterns, materials, texture, color) in the setback area between the building and the property line shall be consistent with and/or match the existing and/or planned pavement treatment of the adjacent public space and/or sidewalk.
- (11) New development shall maintain a compatible relationship with the surrounding development relative to architectural styles, façade design elements, build-to-lines, building mass, scale and height.



Temple Terrace Downtown Community Redevelopment Area
 City of Temple Terrace City Limits 5-6 Story Bldg Height Zones
Street Hierarchy
 Retail Pedestrian Primary Secondary



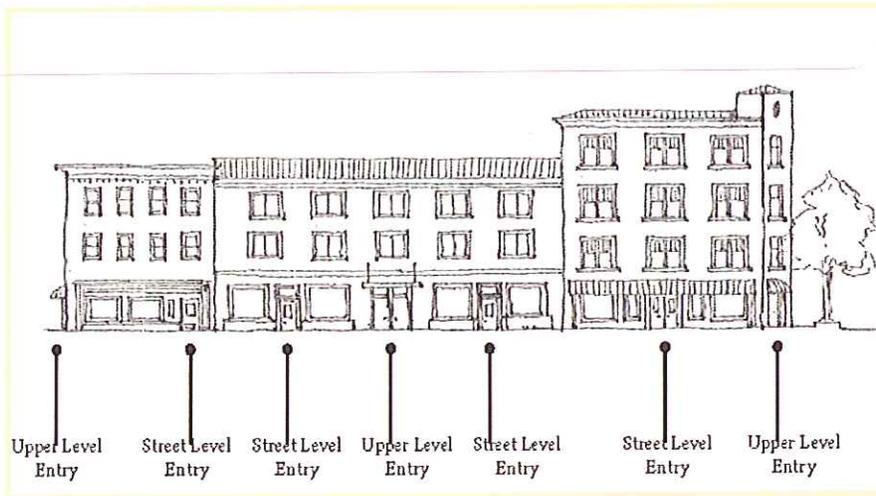
Exhibit V
Building Frontage Street Hierarchy



Street Oriented Building



Clearly Articulated Ground Floor Levels

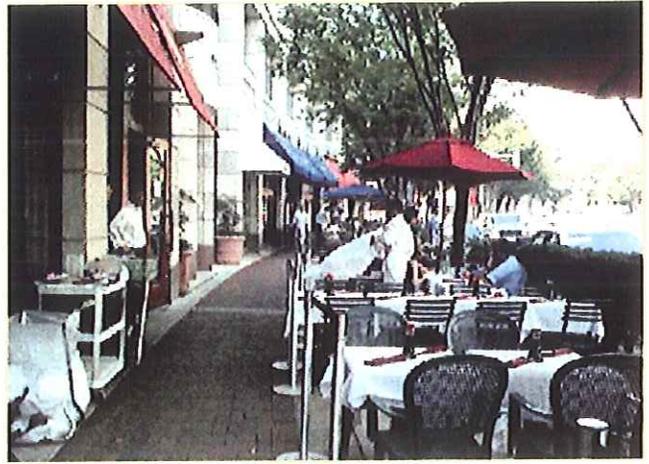


Preferred Facade Section

Exhibit VI
Building Facade Examples



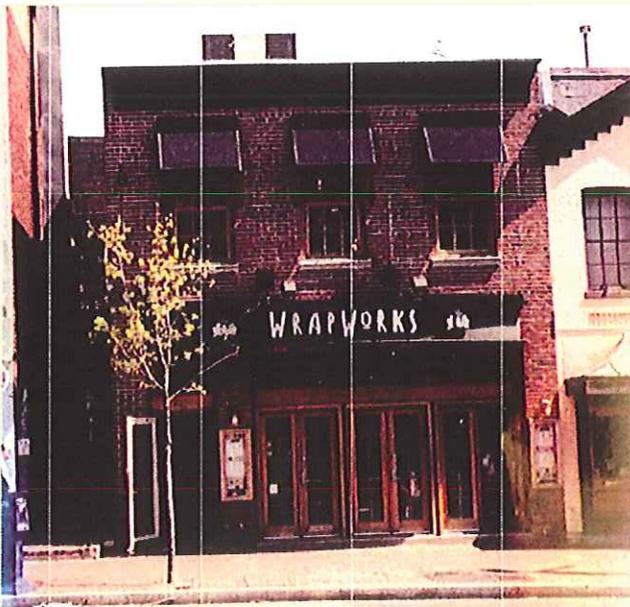
Retail Fenestration



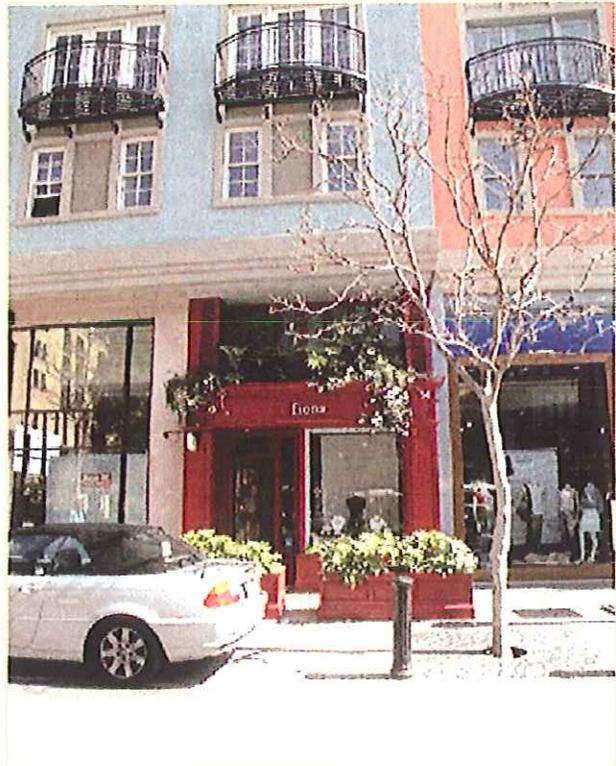
Awnings are permitted above retail use



Retail Fenestration

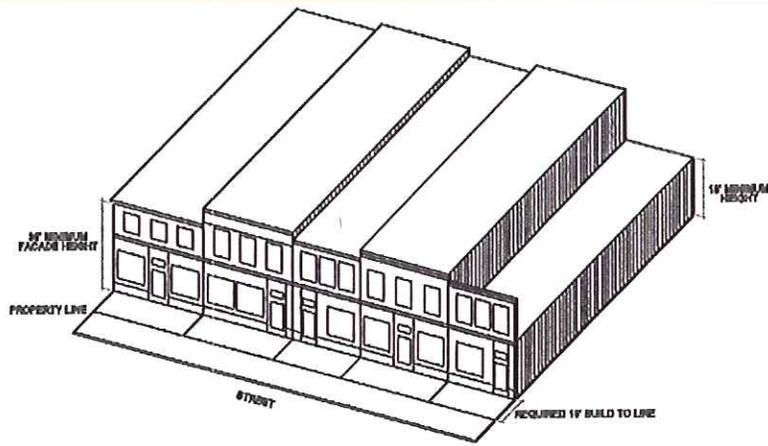


Retail Fenestration

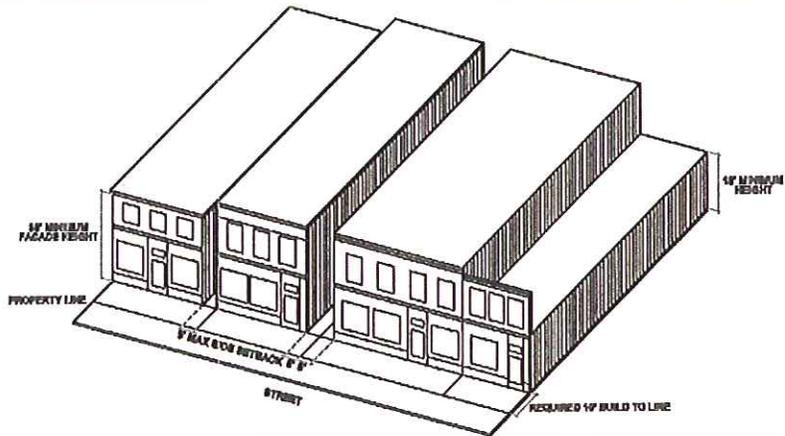


Retail Facade

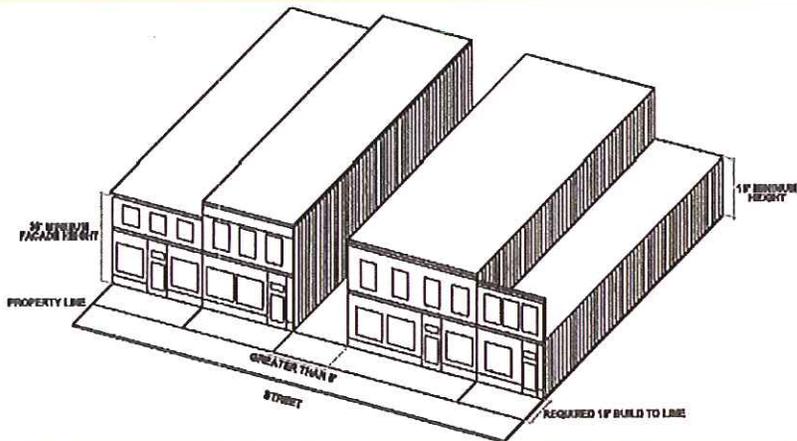
Exhibit VII Ground Floor Facade Examples



APPROPRIATE: FACADES CREATE A CONTINUOUS LINE OF STOREFRONTS



ACCEPTABLE: THE SMALL SIDYARD SETBACK VISUALLY MAINTAINS A CONTINUOUS BUILDING FRONTAGE



INAPPROPRIATE: LARGE SIDYARD SETBACKS DO NOT VISUALLY MAINTAIN A CONTINUOUS BUILDING FRONTAGE

**EXHIBIT VIII
APPROPRIATE/ INAPPROPRIATE COMMERCIAL
STREET FRONTAGE DEVELOPMENT**

TEMPLE TERRACE CODE
CHAPTER 29 - TEMPLE TERRACE DOWNTOWN COMMUNITY REDEVELOPMENT PLAN
OVERLAY ZONING DISTRICT

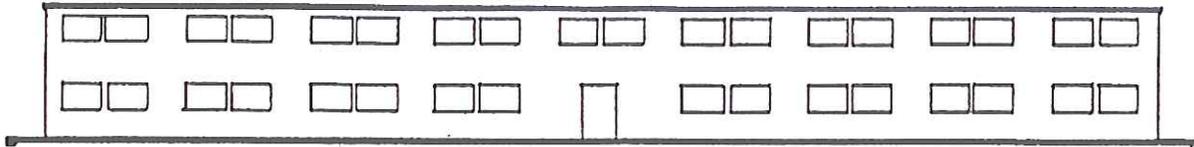
(c) The design standards described below and illustrated in Exhibits VI, VII, VIII and IX shall apply to all Primary and Secondary Streets:

- (1) Building masses and edges along the street shall be articulated with windows, store or office fronts, green yards/porches (residential) and entryways oriented to the sidewalk and the street. Variations in exterior building wall plane in depth or articulation (such as use of columns) is encouraged.
- (2) Buildings that are part of a unified development shall line along the street to create a continuous building frontage along each street. At least seventy (70) percent of the total street frontage shall have a continuous building frontage.
- (3) A building that is on a single parcel and not defined as part of a unified development is encouraged to create a continuous building frontage with the adjacent parcels by having zero (0) side yard setbacks (Exhibit VIII). However, such building may have side yard setbacks of not greater than five (5) feet from the property line, except as provide for in Section 25.925.6- Parking Lots and Garages and Loading Areas.
- (4) The distinction between ground floor level and upper floor levels shall be clearly articulated.
- (5) Blank building walls are prohibited on the street frontage and where side or rear walls of buildings are visible from the street. Where side and rear walls are not visible from the street blank walls are discouraged
- (6) Buildings shall meet the build-to-line, which supersedes any zoning district front or street side yard setback requirement. The build-to-line shall be ten (10) feet from the property line. Religious and educational institutions existing within the Temple Terrace Downtown Community Redevelopment Plan area at the time of the adoption of this Chapter are exempt from this standard.
- (7) Buildings shall be designed for retail, office, residential, recreational or cultural uses at the ground floor. The ground level façade shall have architectural fenestration that creates a pedestrian-scale streetscape. Where the ground level has retail and/or office uses, the façade shall have a minimum (50) percent of its total area in transparent glazing up to a height of fourteen (14) feet. The minimum ground floor ceiling height shall be fourteen (14) feet. The minimum building frontage height shall be two (2) stories or twenty-five (25) feet. The maximum percentage of glazed area that may be blocked by interior fixtures or signage along any street front is thirty (30) percent.
- (8) Residential projects with multiple or row type buildings shall provide separations and/or changes in building plane (façade and roof) to avoid the long barracks like quality imparted by flat long continuous walls and roofs (Exhibit IX).
- (9) Commercial development shall provide pavement finish (for example, patterns, materials, texture, color) in the setback area between the building and the property line consistent with and/or match the existing and/or planned pavement treatment of the adjacent public space and/or sidewalk.
- (10) New development shall maintain a compatible relationship with the surrounding development relative to architectural styles, façade design elements, build-to-lines, building mass, scale and height.

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Section 29.295. 4 BUILDING HEIGHT STANDARDS.

- (a) No buildings shall exceed six (6) stories or 80 feet in height, whichever is more restrictive.
- (b) Buildings that are five (5) or six (6) stories in height shall be only permitted within one-hundred (100) feet from where Retail Pedestrian Streets intersect or where a Retail Pedestrian Street and Primary Street intersect (Exhibit V). Waivers may be granted to this provision for unified developments.
- (c) Where adjacent buildings are of different heights, design techniques that architecturally align façade elements and details through the use of such alignment features as balconies, windows, store fronts, or horizontal string courses shall be employed. The purpose of establishing architectural alignment between adjacent buildings is to establish a compatible and harmonious design relationship with adjacent and nearby buildings, and further, to establish and/or maintain a pedestrian-scale streetscape, as illustrated in Exhibit X.
- (d) The maximum height of buildings adjacent to a single-family residential district shall be limited to two (2) stories or 35 feet in height, whichever is more restrictive.
- (e) The maximum height of buildings across the street from a single-family residential district shall be limited to two (2) stories or 35 feet in height, whichever is more restrictive.
- (f) Buildings of five (5) and six (6) stories in height shall utilize tapering and step back design techniques that reduce building bulk as building height increases (Exhibit X).
- (g) Exceptions to the height limits are appurtenances, trellised roof decks and other similar structures covering not more than twenty-five (25) percent of a building's roof to which the structure is attached. These structures may exceed the maximum permitted height limits by up to ten (10) feet.

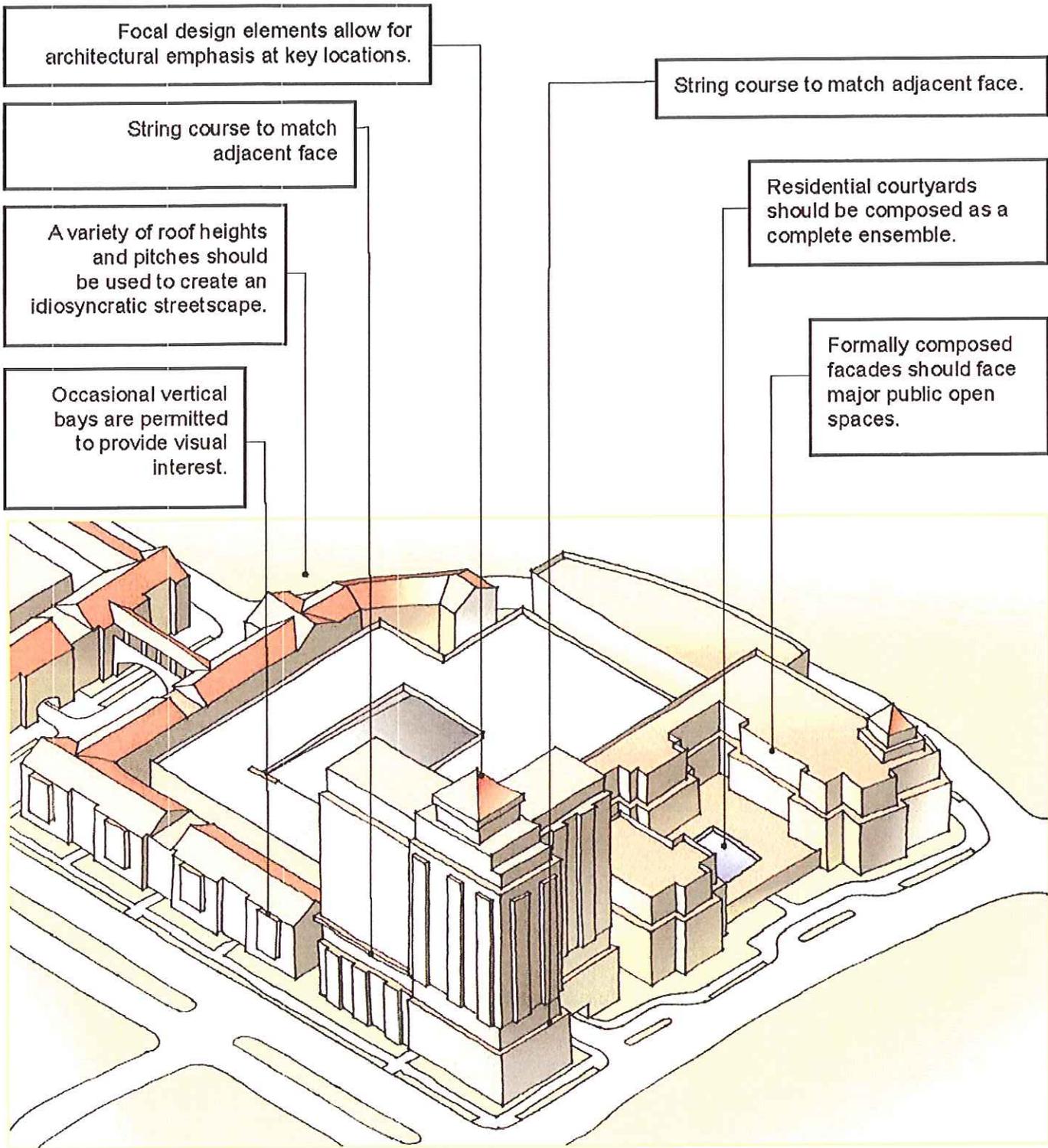


INAPPROPRIATE: THE LARGE BARRACKS LIKE STRUCTURE IS FLAT, VISUALLY BORING AND UNAPPEALING.



APPROPRIATE: AN ALTERNATIVE TO A LARGE BUILDING IS THAT OF A CLUSTER OR GROUPING OF BUILDINGS. ARCHITECTURAL STYLING AND DETAILING MAINTAIN THE COHESIVENESS OF THE DEVELOPMENT WHILE VARIED BUILDINGS AND DESIGN OFFERS A GREAT DEAL OF VISUAL RELIEF AND INTEREST. THE VISUAL SCALE OF THE DEVELOPMENT IS ALSO SIGNIFICANTLY REDUCED.

Exhibit IX
Residential Building Articulation



Exemplary Building Height

Exhibit X
Building Height Compatibility

TEMPLE TERRACE CODE
CHAPTER 29 - TEMPLE TERRACE DOWNTOWN COMMUNITY REDEVELOPMENT PLAN
OVERLAY ZONING DISTRICT

Section 29.925. 5 ACCESSORY STRUCTURES AND SITE FEATURES.

- (a) Accessory structures and site features shall meet the following design guidelines:
- (1) Accessory structures and equipment shall be located in the rear yard or at the rear of the commercial building, and in the rear or side yard or at the rear of the residential building. Accessory structures such as sheds shall be the same architectural style, materials and color as the principal structure. Equipment such as heating and air-conditioning machinery, transformers and generators shall be screened with a wall, fence or landscaping in such a manner that the equipment is not visible to the public view.
 - (2) Any required fencing or walls shall be decorative and compatible with the architectural style of the principal building and match the principal building in materials and color. Decorative masonry wall finishes and/or wrought iron are appropriate design materials for Spanish Renaissance Revival and Mediterranean Revival architectural style buildings. Masonry walls, PVC fences and/or wrought iron details/fences are appropriate design materials for St. Augustine Spanish Colonial architectural style buildings. Wood, wood stockade and chain link fences are not appropriate fencing materials, and therefore, are not permitted.
 - (3) Fence or wall planes shall provide for visual variations in the fence/wall plane. Therefore, wall/fence planes shall not extend more than fifty (50) feet without a significant variation in the fence/wall plane through the use of columns or patterned breaks in the fence/wall plane, and use of landscaping.
 - (4) Dumpsters shall be enclosed with an opaque wall or opaque fencing that matches the materials and colors of the principal structure.
 - (5) Site features such as stormwater ponds shall be located to the rear of the site where possible. Stormwater ponds are not permitted in the front yard, build-to-line area or along any street front. Ponds may be located in the side yard provided the perimeter of the pond has a minimum five (5) foot wide landscaped buffer (trees and continuous hedge). Any required fencing or wall shall be a maximum of four (4) feet in height, located behind the landscaping and shall meet the fence/wall design requirements of this Chapter.

Section 29.925. 6 PARKING AREAS, LOADING AREAS and PARKING REQUIREMENTS

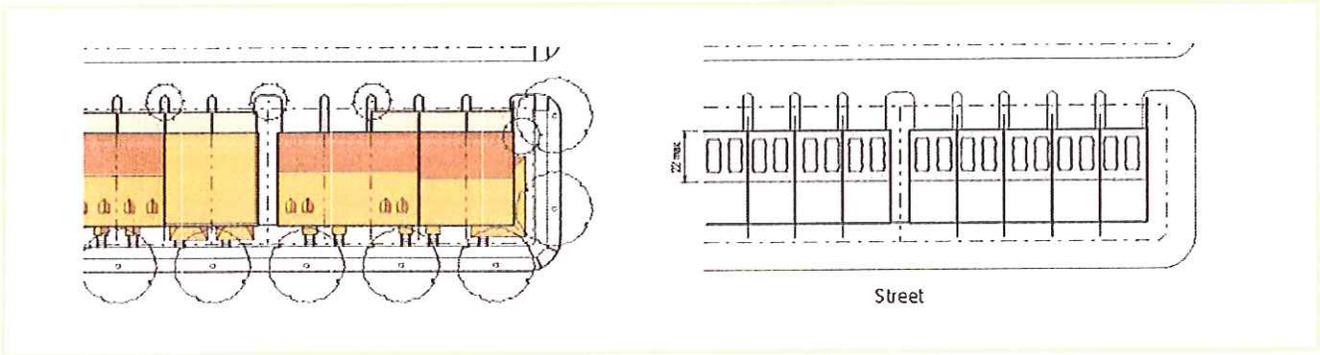
- (a) Parking lots, parking garages and loading areas shall meet the following locational criteria and design guidelines (Exhibit XI):
- (1) Parking lots, parking garages and loading areas shall be located to the rear of buildings fronting along Retail Pedestrian Streets and to the rear or side of buildings fronting along Primary and Secondary Streets. Access to parking and loading areas shall be via the Secondary Street or alleyway. Driveways along Retail Pedestrian, Primary and Secondary Streets existing at the time of the adoption of this Chapter are required to be removed at the time substantial expansion or redevelopment occurs on the parcel in order to conform with the access requirements of this Section.

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OVERLAY ZONING DISTRICT

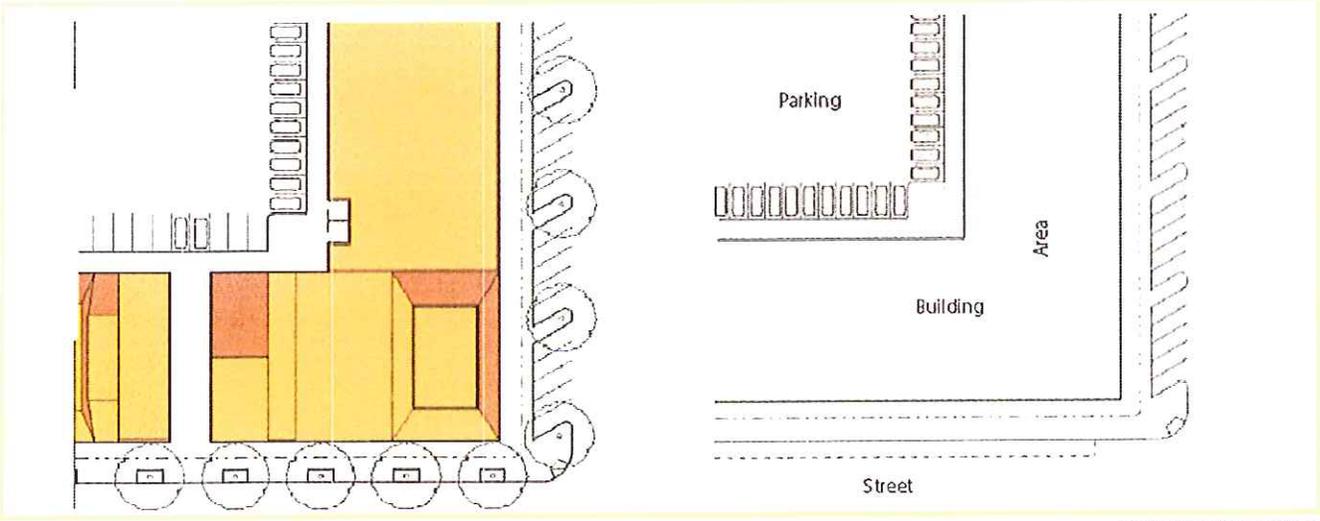
- (2) In cases where access to a parking lot, garage or loading area is not available via an alleyway or Secondary Street, access to the parking/loading area shall be located along the side yard or at the side of the building, but no parking or loading is permitted in front of the building or within the street frontage build-to-line area. The access driveway area (driveway and landscaping) shall not exceed thirty (30) feet. A parking lot (access driveway and limited parking, but no loading) that fronts along a Primary or Secondary Street shall not exceed more than fifty (50) percent of the total lot street frontage or a minimum of thirty (30) feet, whichever is greater. All parking lots including required landscape buffers that front along a Retail Pedestrian, Primary or Secondary Street shall not be located within the build-to-line area.
- (3) Parking garages may front along a Primary or Secondary Street provided the ground floor frontage is designed to accommodate commercial, office and/or recreational/cultural uses. All parking garages shall have decorative facades on all parking levels that front along streets, and such façades shall be designed in one of the architectural styles described in Section 29.925. 2. All parking garages shall meet the design guidelines set forth in Sections 29.925.3 and 29.925.4.
- (4) Uses with drive-through service windows shall be limited to the side of buildings, or to the rear of buildings and accessed via Secondary Streets or alleys provided they do not substantially disrupt pedestrian activity or surrounding uses. Drive through service windows shall be accessible by bicyclists.

(b) Within the Downtown Community Redevelopment Area there shall be established a maximum number of parking spaces permitted within a development to ensure compliance with the DMU-25 plan category and promote pedestrian oriented development. The maximum parking permitted for residential, retail and office is as follows, all other uses shall comply with the minimum standards contained in Chapter 25:

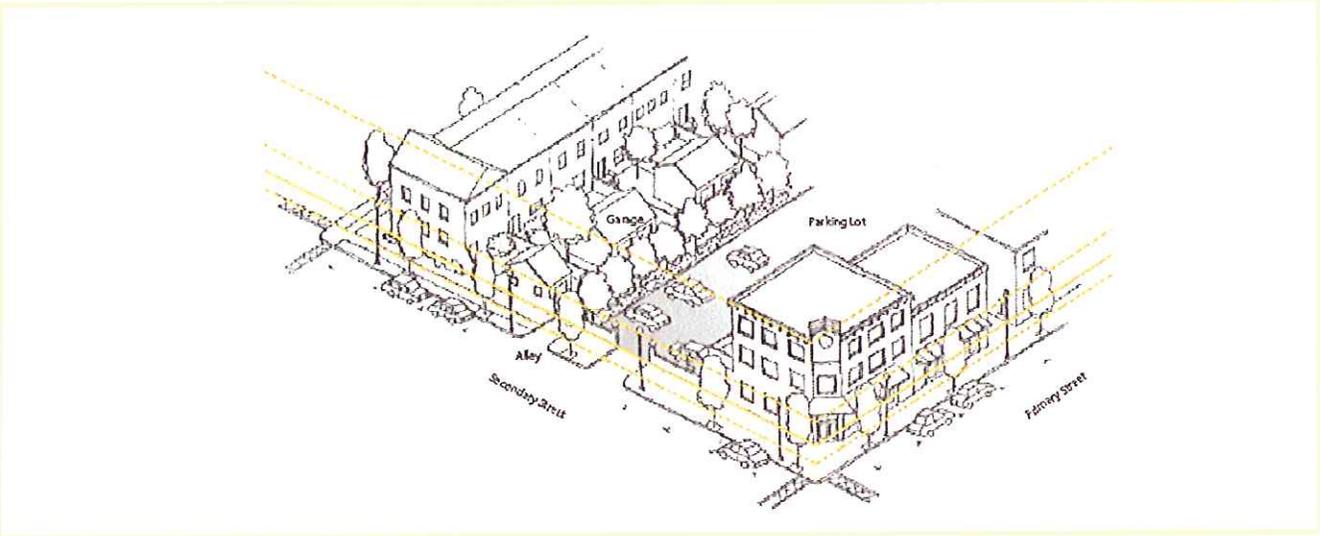
- (1) Residential 3 bedrooms or more: 2.2 spaces/du.
- (2) Residential 2 bedrooms or less: 1.6 spaces/du.
- (3) Retail in Mixed-Use Building: 4 spaces/1,000 gross square feet.
- (4) Free-Standing Retail: 4.5 spaces/1,000 gross square feet.
- (5) Office in Mixed-Use Building: 3 spaces/1,000 gross square feet.
- (6) Free-Standing Office: 3.5 spaces/1,000 gross square feet.



Parking Under Building



Parking Behind Building



Example of Fourth and Second Layer Parking

Exhibit XI
Parking Locations

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CHAPTER 29 - TEMPLE TERRACE DOWNTOWN COMMUNITY REDEVELOPMENT PLAN
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Section 29.925.7 SIGNS.

(a) Only ground and wall signs shall be permitted, except for signs not requiring a permit pursuant to Chapter 25. One ground sign is permitted pursuant to Chapter 25 for the C-G zoning district. In addition, one wall sign is permitted up to a maximum of twenty (20) percent of the wall area of the building façade, but not to exceed a maximum of 125 square feet in sign area. Wall signs shall be placed on flat, unadorned parts of the building façade, such as the horizontal band between the store front (ground floor) and second floor or the equivalent wall sign area may be placed on awnings.

(b) At the time of sign permit application or as part of the site plan review process, all projects shall develop a unified sign program for review and approval. The sign program shall address the coordination of the size, style, number, type and color scheme of all the signs to be erected on the site. (These regulations are not meant to regulate or approve the contents of the signage.) Signage for individual sites, while designed to satisfy the objective of expressing individual corporate image, must also be of a character and size consistent with the overall sign program. The following standards shall be incorporated in all new or modified signs:

- (1) All signs within a project shall be consistent in color, size, type and lettering style (font).
- (2) The property address shall be included on the project's wall or ground sign.

Section 29.925.8 SITE LIGHTING.

The following light standards shall be incorporated into all non-residential site improvement plans:

(a) Ornamental poles shall be manufactured by Sternberg Vintage Lighting, or the equivalent thereof; ornamental poles shall be "Augusta Series," Model 4212-FP4, 12" in height, standard finish-black, or the equivalent thereof. (Section 29.935 (h) Exhibit XII)

- (1) Poles shall be required only along Retail Pedestrian and Primary Streets where site plans are submitted for approval. This includes existing properties that are modified and required to follow the site plan approval process.
- (2) Poles shall be spaced 60' on center maximum, or one pole per 60 linear feet of roadway frontage.
- (3) Use of matching ornamental poles and light fixtures shall be encouraged for the interior portions of a site (i.e. parking lots).
- (4) Poles shall be installed on concrete base pad. Base pad shall be circular.

(b) Lighting fixtures shall be manufactured by Sternberg Vintage Lighting; fixtures shall be "Old Town," Model A850/508 polycarbonate acorn, or the equivalent thereof.

- (1) Lamping: Metal Halide, 175 watt.
- (2) Socket type: Mogul Base.
- (3) Finial: Four vaned, black (standard with fixture).
- (4) Optic systems: Glass Refractor, Type III.
- (5) Fitters: 508 Series fitter with ballast compartment.

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- (c) Miscellaneous lighting accessories include:
- (1) Banner arm – Single banner arms (18” x 36”) shall be specified for each pole located adjacent to the right-of-way. Banner arms are not required for internal site lighting.
 - (2) Single convenience receptacle – convenience receptacle shall be required for all poles.
 - (3) Photocells – all street lighting shall utilize photocells to automatically control lights.
 - (4) Bollard lighting – “Augusta” 4201-LB (optional).
 - (5) Wall fixtures – “Old Town” A840 on No. 50 WB (optional).
 - (6) Street sign arm (optional).

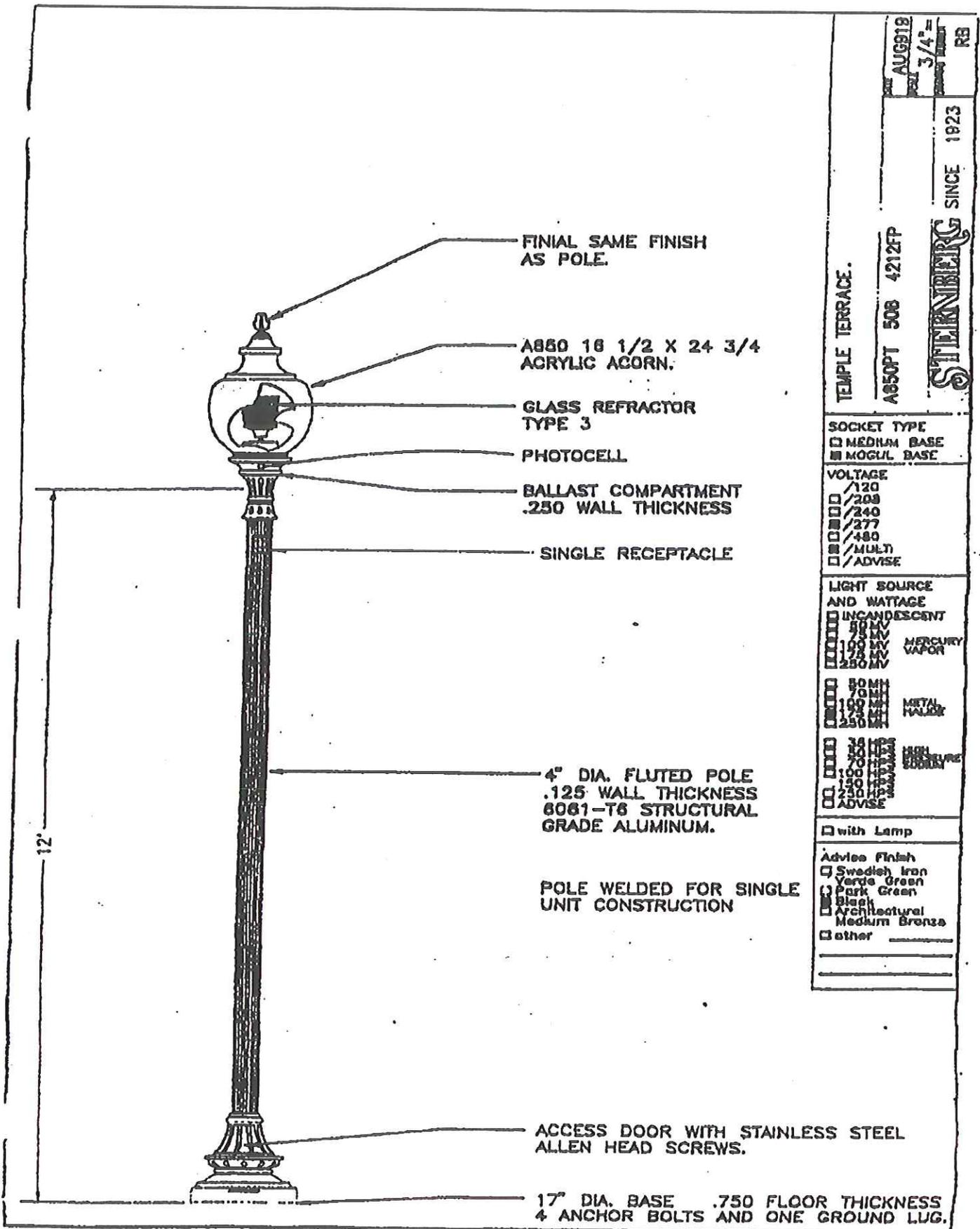


Exhibit XII LIGHTING

TEMPLE TERRACE CODE
CHAPTER 29 - TEMPLE TERRACE DOWNTOWN COMMUNITY REDEVELOPMENT PLAN
OVERLAY ZONING DISTRICT

Section 29.925.9 PUBLIC AND PRIVATE STREETSCAPE AND OPEN SPACES

(a) Street planting shall be installed along all major thoroughfares. Trees, shrubs and ground cover will be planted along 56th Street and Busch Boulevard. Landscape materials recommended are: Palms along the arterial streets and seasonal annuals planted between the curb and the sidewalks at major intersections, and planter beds with low flowering shrubs (dwarf oleander or plumbago) used as color accent beds along arterial planting strips. Streetscape improvements are shown Exhibit XIII.

(b) Any proposed or required landscaping for private development that fronts along a street and/or is located within parking areas that front along a street pursuant to Section 29.925.6 shall be compatible with the streetscape landscape plan shown in Exhibit XIII by incorporating palm trees such Royal Palm, Queen Palm, Date Palm or other similar type/scale palm trees in the design.

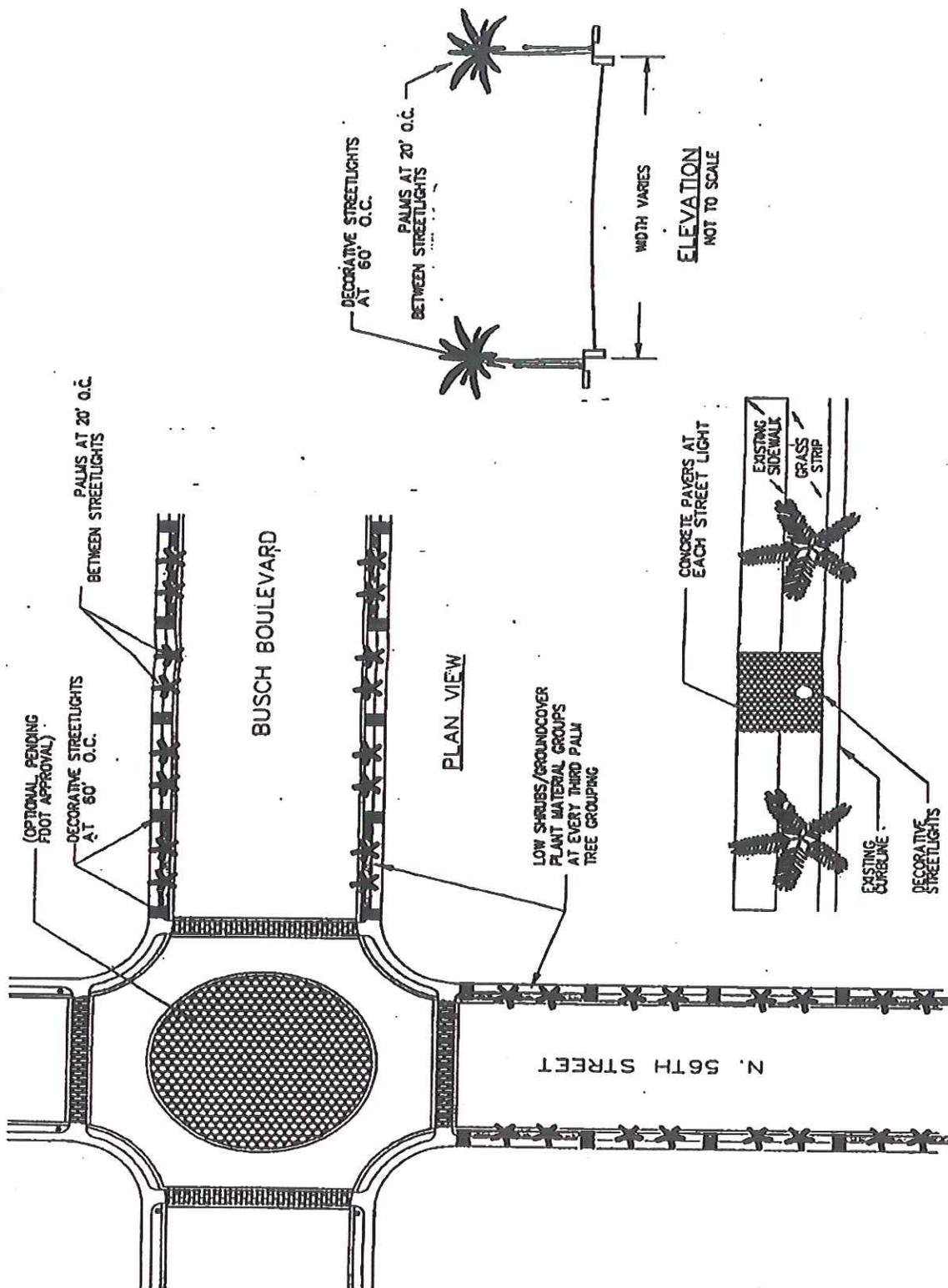
(c) Private and public open spaces shall be located so as to provide for physical and visual connections to adjacent street/public sidewalk networks. Such open spaces shall include the following design features: pedestrian path hardscape materials (brick or stone pavers), water features (fountains, pools, artwork), artwork, and landscape areas (Exhibit XIV).

(d) Where open spaces abut Retail Pedestrian Streets, open space areas shall provide seating areas and shade (canopies, tables with umbrellas, shade trees and/or awnings) (Exhibit XIV).

(e) To the greatest extent possible, private and public open spaces shall interconnect physically to create an interconnected system of open spaces throughout the Downtown Redevelopment Area.

Section 29.925. 10 COLOR PALETTE.

(a) New development or substantial expansion of existing development shall use the color palette, found in Exhibit XV, as a guide and/or use colors appropriate for Spanish Renaissance Revival, Mediterranean Revival or St. Augustine Spanish Colonial architectural styles.



STREETSCAPE IMPROVEMENTS
FOR MAJOR ROADWAYS
CITY OF TEMPLE TERRACE

DETAIL ENLARGEMENT

Exhibit XIII - Streetscape Landscaping

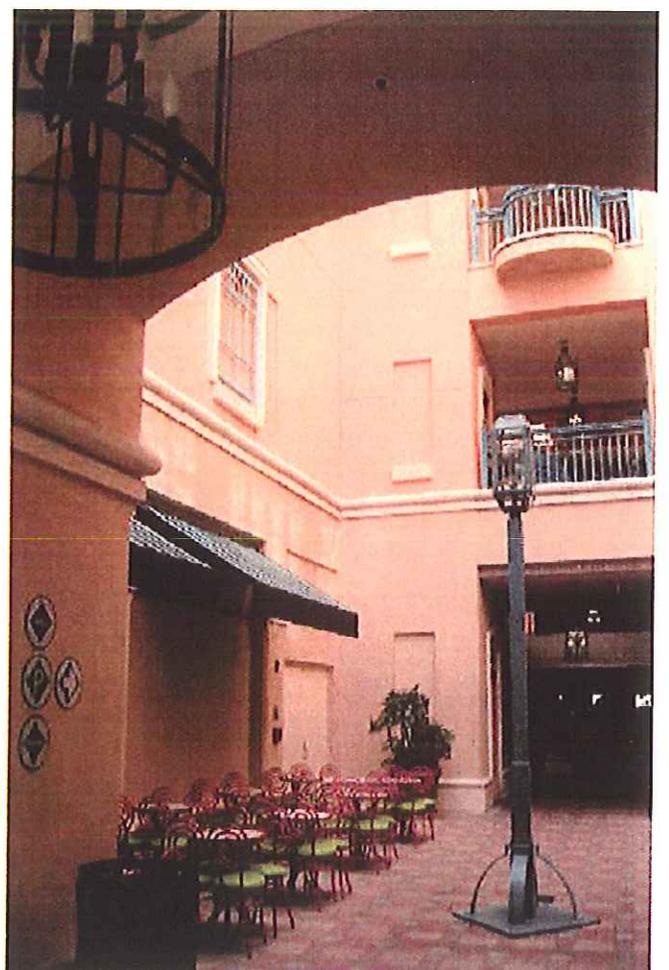
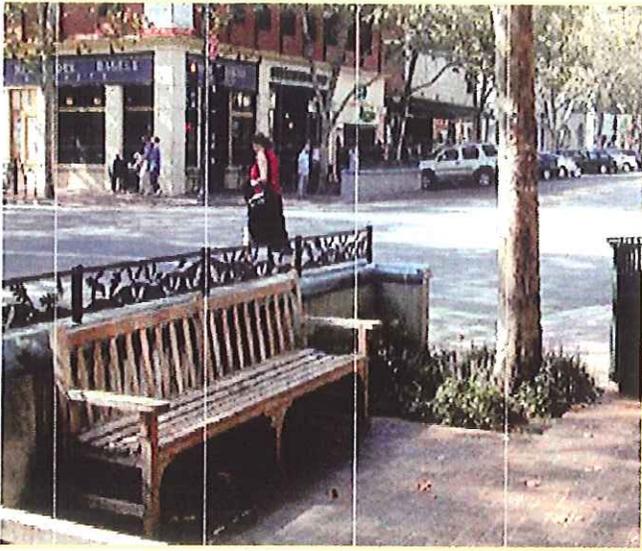
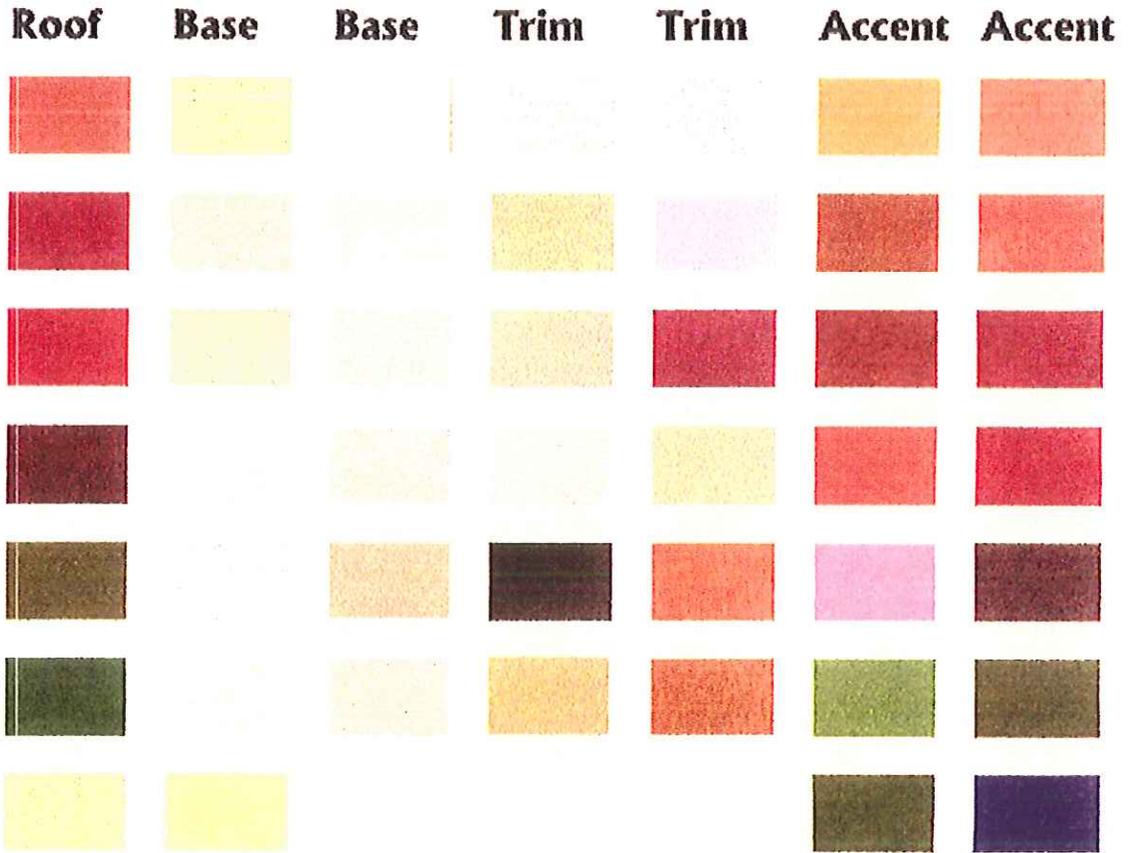


Exhibit XIV
Open Space Design Examples



Pantone Color Numbers

Roof	Base	Trim	Accent
1385 O	4545 VVLBr	400 VLGr	117 Br
1595 DO	461 VLYG	4515 LBR	1385 O
1805 DR	468 VVVLBr	4525 VLBr	154 Br
1807 DR	467 VVLBr	466 VLBr	1595 DO
1815 Br	475 WVLO	474 LO	1665 O
5763 Gr	482VVLBr	4775 LBr	174 DR
	600 WVLY	481 VLBr	1777 LR
	607 WVLY	488 LP	1805 DR
	705 VLP	492 Br	1807 DR
	712 WVLO	712 VLO	1815 Br
	719 WVLO	713VLO	370 DG
	726 VVLBr	730 LBr	371 DG
	No Pantone equal		408 Gr
	White		410 Gr
	Off-white		411 Gr
			5763 Gr

KEY	Y = Yellow	Bl = Black
V = Very	R = Red	Gr = Grey
L = Light	B = Blue	Br = Brown
D = Dark	G = Green	P = Pink
	O = Orange	

Exhibit XV
Color Palette

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Section 29.930 DEFINITION OF TERMS.

Unless specifically defined below or as defined in Chapter 25, words or phrases used in this Chapter shall have the meaning of common usage which gives this Chapter its most reasonable application:

Architectural High Style is a more refined and formal architectural design style based on its historical origins that is predicated upon formal and specific rules within a particular style and is characterized by a higher level of composition and design details. Illustrations of the design elements of the Temple Terrace Mediterranean Revival high styles are contained in Section 29.925.2 Exhibits III A - III C.

Architectural Vernacular Style is a style that developed "naturally" in response to local climate, social, and economic factors over time. This architecture creates a more informal character and may be symmetrical or, more typically, asymmetrical in its composition, but exhibits less refined details than the High Style. Illustrations of the design elements of the Temple Terrace Mediterranean Revival vernacular styles are contained in Section 29.925.2 Exhibits III A- III C.

Blank Building Wall is defined as any building wall where transparent glazing is equal to twenty-five (25) percent or less of the wall area.

Build-To-Line is a line parallel to the property line, along which a building or structure shall be built as specified by the classification of the adjacent street.

Building Frontage is comprised of the front and street side yard setback areas and the building façade. The building frontage includes the dimensional depth of the building front yard setback, the portion of the building façade along the front yard and the architectural elements that make up the facade such as architectural style details, colonnades, shop fronts, stoops and entrances or balconies.

Compatibility is measured by how the design of a building or project relates to the design elements of the surrounding natural/physical and man-made environments. Compatibility measures include but are not limited to building scale, mass and form; building relationship to the street; transitions in building height and; the use of building materials and harmonious reoccurrence of significant façade design features such as window patterns/rhythms, cornice lines and roof lines.

Design Elements are the features of a building which includes architectural style, roof and façade details (cornices, arches, building materials), the rhythm (pattern and spacing) and proportion of windows, porches, doors, and vertical and horizontal features (columns, lintels).

Fenestration is the arrangement of windows, doors and other exterior openings on a building.

Focal Design Elements are design features such as towers, cupolas, dormers, parapets or other unique roof features that create a visual emphasis particularly at street corners to the vertical elements of a building.

Mediterranean Revival Style is defined and illustrated in Section 29.925.2 Exhibits III-B.1 and III-B.2.

Retail is defined as any building that is used or designed and constructed for the display and sale of products purchased on a frequent basis. Products sold may include, but not be limited to, household goods, groceries, furniture, appliances, sale of food and/or beverages (consumption on-site and/or take-out), home entertainment equipment, specialty retail, retail banking, art, jewelry, florists, apparel and footwear, books and stationary, appliances, and camera and art supplies.

St. Augustine Spanish Colonial Style is defined and illustrated in Section 29.925.2 Exhibit III-C.

TEMPLE TERRACE CODE
CHAPTER 29 - TEMPLE TERRACE DOWNTOWN COMMUNITY REDEVELOPMENT PLAN
OVERLAY ZONING DISTRICT

Spanish Renaissance Revival Style is defined and illustrated in Section 29.925.2 Exhibits III-A.1 and III-A.2.

Substantial Expansion of Existing Development as used in this Chapter shall be defined as any expansion or redevelopment of an existing site and structure that exceeds fifty (50) percent of its current valuation (land and improvements) as identified by the Hillsborough County Property Appraiser at the time a development application is submitted to the City . Further, substantial expansion shall also mean any cumulative expansion or redevelopment of an existing site and structure that exceeds fifty (50) percent of its current valuation over a consecutive two year period.

Unified Development is defined as a group of buildings that are either under single ownership or are reviewed by site plan or PUD as a single project.